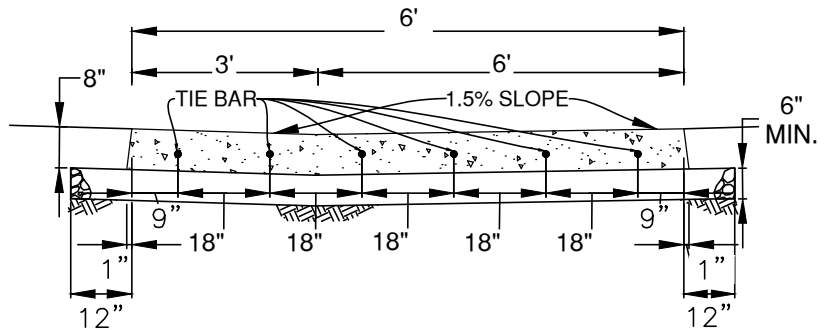
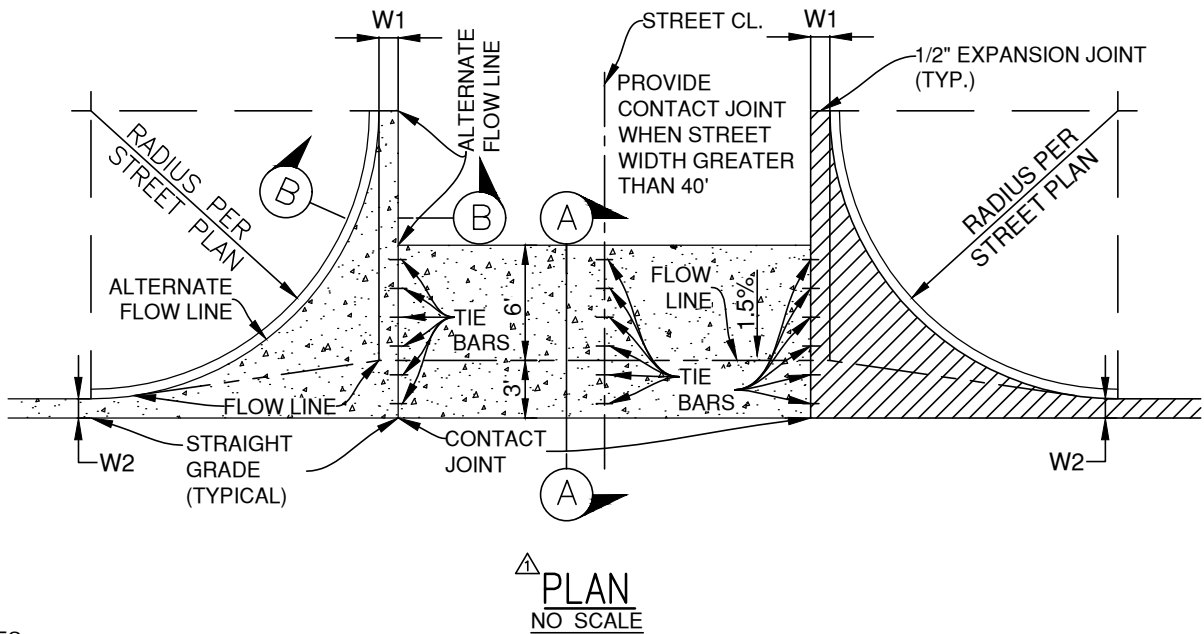


**SECTION B-B**



**SECTION A-A**



**PLAN**  
NO SCALE

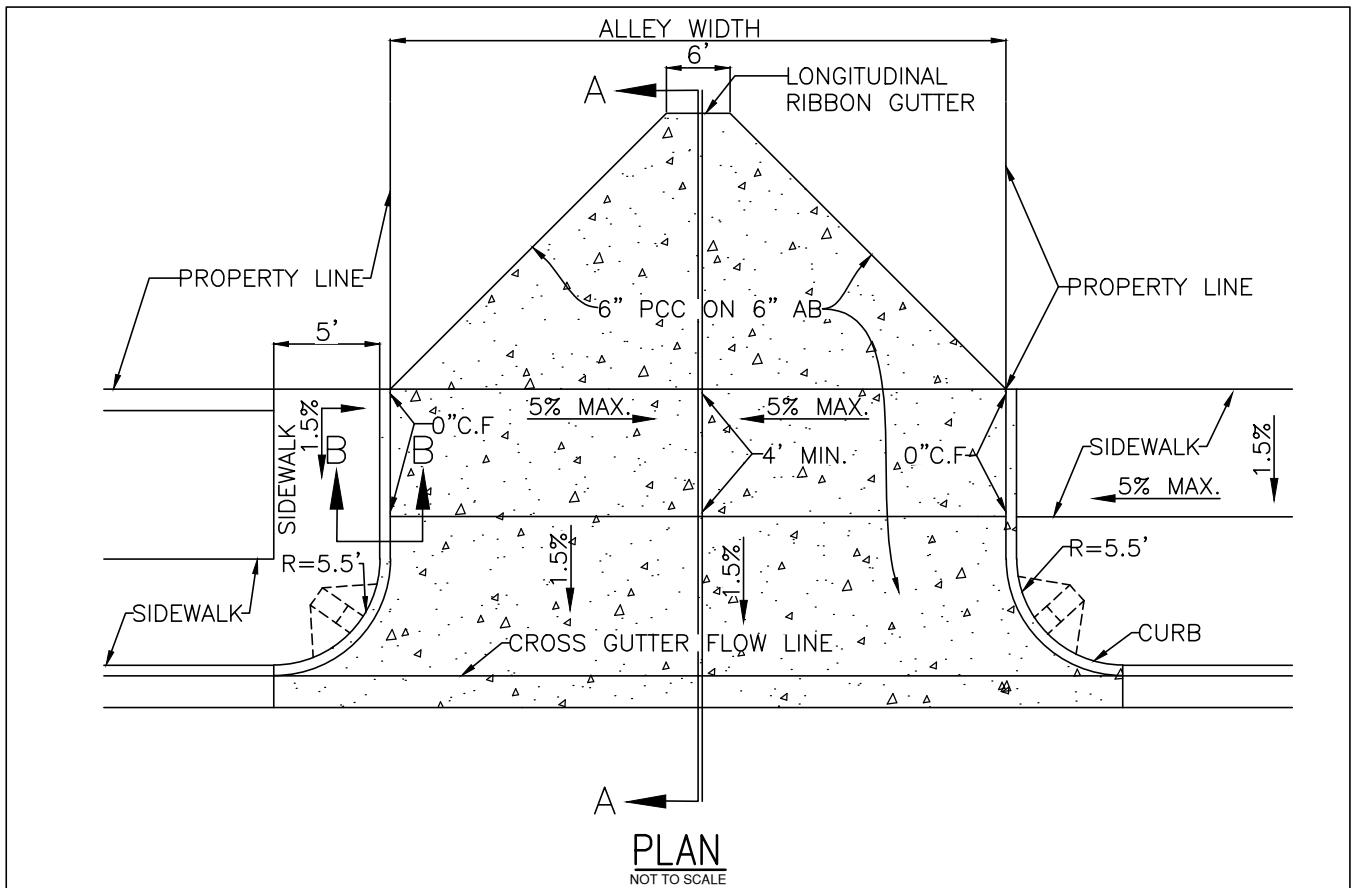
**NOTES:**

1. WEAKEN PLANE JOINT TOOLED 1 1/2" DEEP SHALL BE SUBSTITUTED FOR CONTACT JOINT WHERE MONOLITHIC CONSTRUCTION IS APPROVED BY THE CITY ENGINEER.
2. CROSS GUTTERS SHALL BE CONSTRUCTED OF CLASS 517 C 2500 PORTLAND CEMENT CONCRETE EIGHT (8") THICK.
3. TIE BARS SHALL BE 3/4" Ø X 18" LONG SMOOTH STL. BARS @ 18" CENTERS, GREASE ONE END.
4. BASE SHALL BE CAB OR CMB PER STD. SPECS. (REV. OCTOBER 2006).
5. PEDESTRIAN ROUTE SHALL BE HAVE A CROSS SLOPE OF 2% MAX.

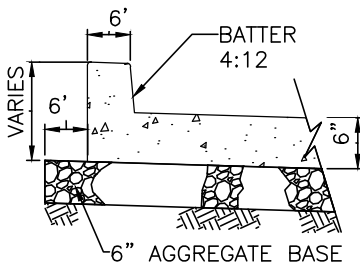
| AREA OF SPANDREL (CROSS-HATCHED) |     |                |                |                |
|----------------------------------|-----|----------------|----------------|----------------|
| W1                               | W2  | R=27'          | R=32'          | R=36'          |
| 18"                              | 18" | 239.69 SQ. FT. | 318.00 SQ. FT. | -              |
| 18"                              | 24" | 253.94 SQ. FT. | 334.75 SQ. FT. | -              |
| 24"                              | 18" | 253.94 SQ. FT. | 334.75 SQ. FT. | -              |
| 24"                              | 24" | 268.44 SQ. FT. | 351.75 SQ. FT. | 426.12 SQ. FT. |

W1 A W2= GUTTER WIDTH PER STREET PLAN

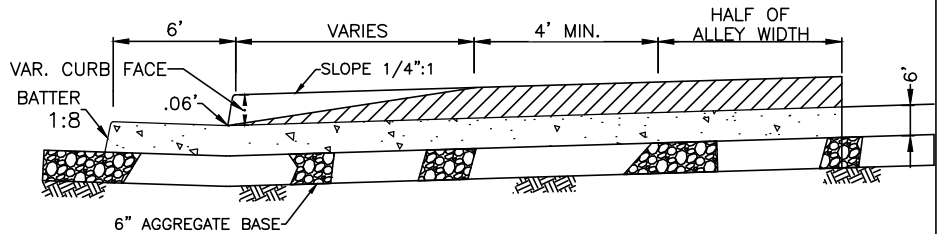
|                                           |          |              |
|-------------------------------------------|----------|--------------|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
| <b>STREET CROSS GUTTER</b>                |          |              |
| DRAWN BY: _____                           | STANDARD | 1<br>OF<br>1 |
| CHECKED BY: _____                         |          |              |
| APPROVED BY: _____                        |          |              |
| DATE                                      | INITIAL  |              |



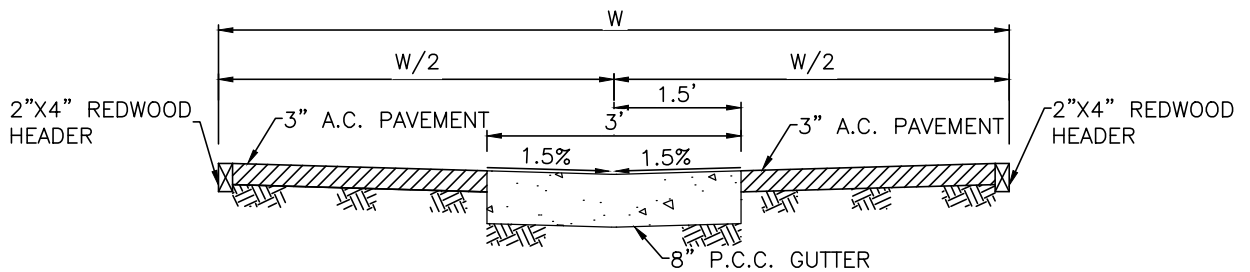
**PLAN**  
NOT TO SCALE



**SECTION B-B**  
NOT TO SCALE



**SECTION A-A**  
NOT TO SCALE

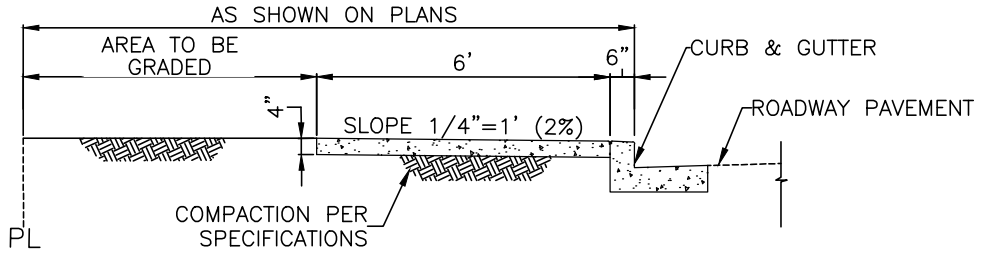


**TYPICAL ALLEY SECTION**  
NOT TO SCALE

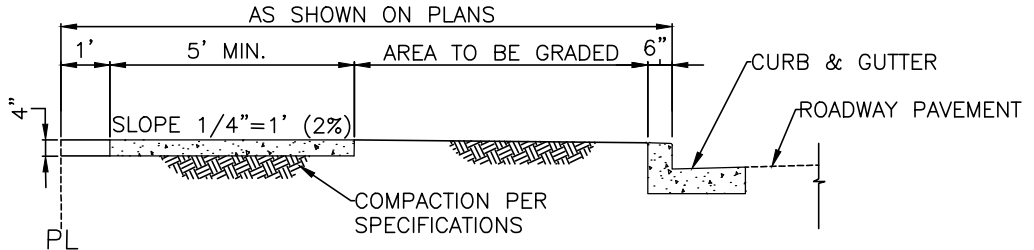
**NOTES:**

1. ALL WORK SHALL BE IN ACCORDANCE WITH " STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION" LATEST EDITION, INCLUDING AMENDMENTS.
2. P.C.C. SHALL BE 520-C-2500.
3. BASE SHALL BE CAB OR CMB.

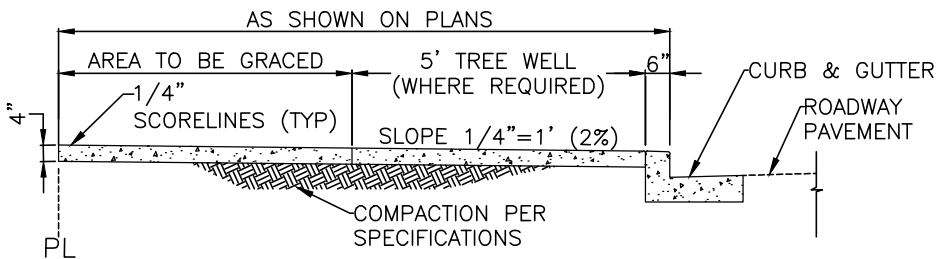
|                                                     |          |              |
|-----------------------------------------------------|----------|--------------|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT           |          |              |
| <b>ALLEY INTERSECTION &amp; ALLEY CROSS SECTION</b> |          |              |
| DRAWN BY: _____                                     | STANDARD | 1<br>OF<br>1 |
| CHECKED BY: _____                                   |          |              |
| DATE INITIAL APPROVED BY: _____                     |          |              |



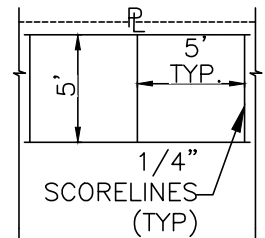
**CASE I – RESIDENTIAL SIDEWALK ADJACENT TO CURB**



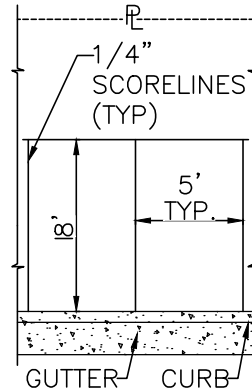
**CASE II – RESIDENTIAL SIDEWALK NOT ADJACENT TO CURB**



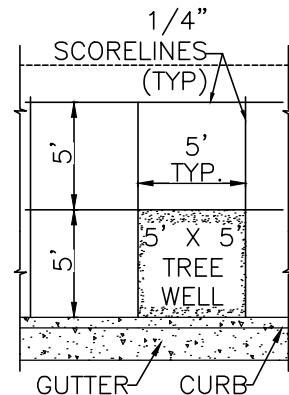
**CASE III – COMMERCIAL FULL WIDTH**



**CASE II**



**CASE I**



**CASE III**

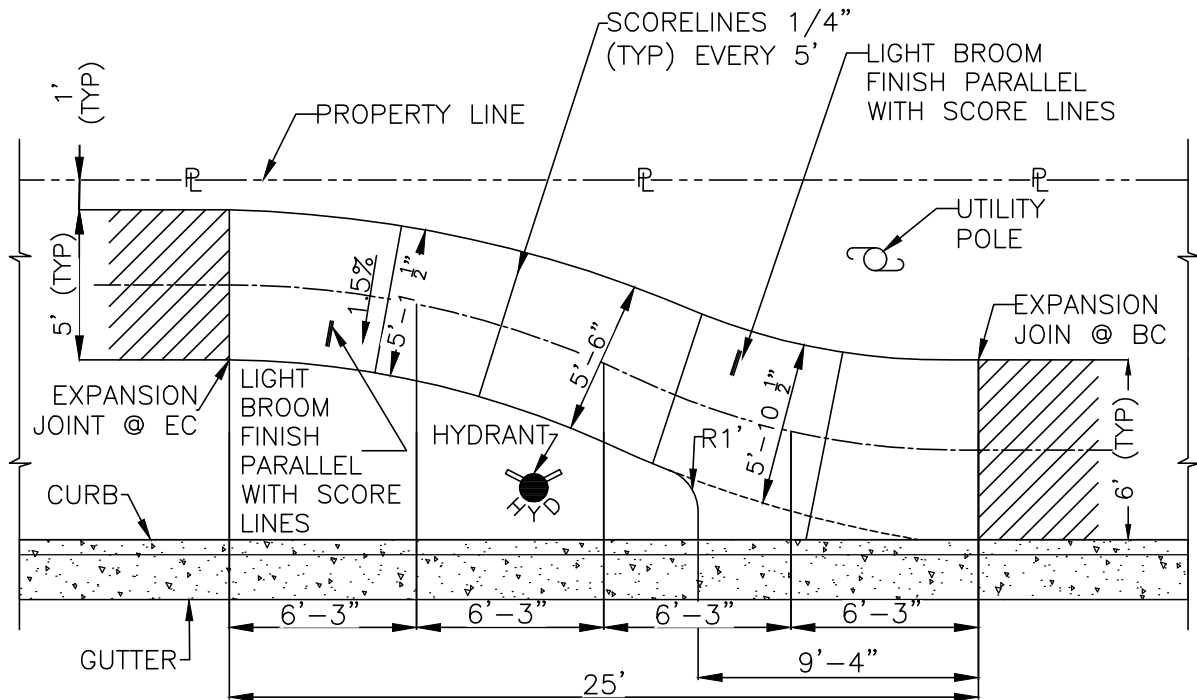
**NOTES:**

1. ALL WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH LATEST ADOPTED STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
2. CONCRETE SHALL BE CLASS 520-C-2500.
3. MISSING SIDEWALK & SIDEWALKS REPAIRS SHALL BE PLACED TO MATCH ADJACENT SIDEWALK WIDTH & FINISH.
4. A LIGHT BROOM FINISH SHALL BE APPLIED PERPENDICULAR TO STREET.
5. COMPACT SOIL BENEATH SIDEWALK TO 90% RELATIVE COMPACTION PER APWA SPECIFICATIONS. PROVIDE SOIL AMENDMENT IF REQUIRED BY CITY ENGINEER.
6. 1/2" THICK FULL DEPTH TRANSVERSE EXPANSION JOINTS SHALL BE PLACED AT CURB RETURNS & AROUND DRAINAGE STRUCTURES, POLES, PIPES, ETC.

CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**SIDEWALK DETAILS**

|   |  |      |         |                    |          |              |
|---|--|------|---------|--------------------|----------|--------------|
|   |  |      |         | DRAWN BY: _____    | STANDARD | 1<br>OF<br>1 |
|   |  |      |         | CHECKED BY: _____  |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____ |          |              |

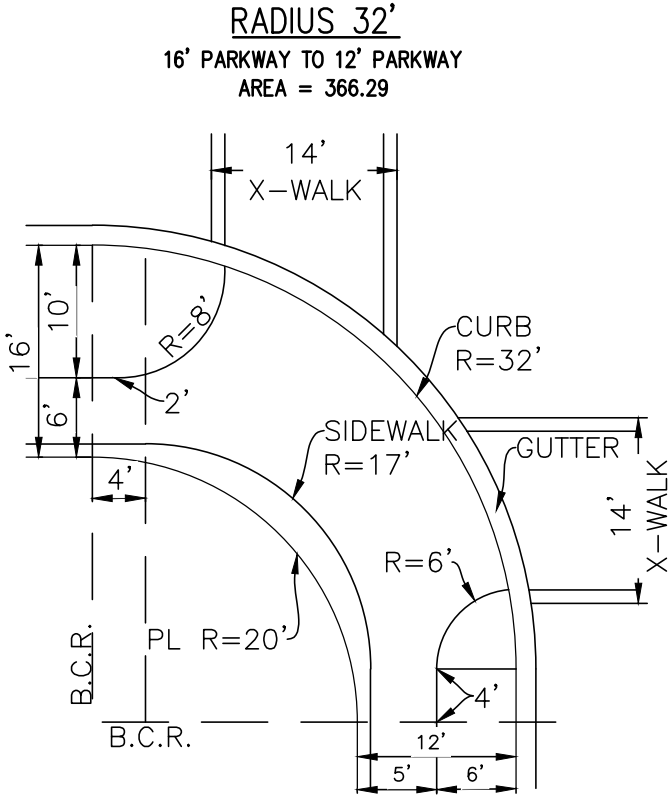
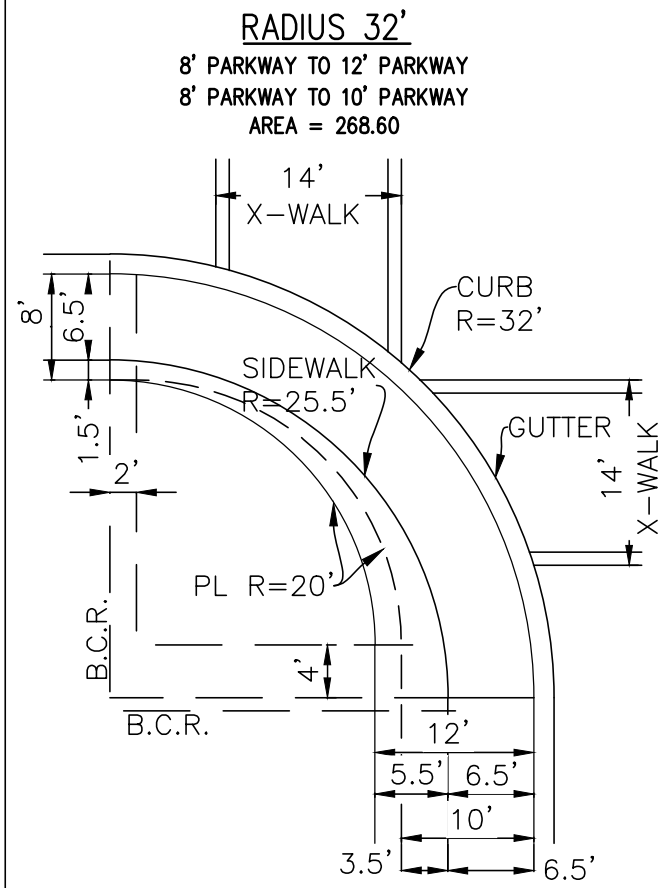
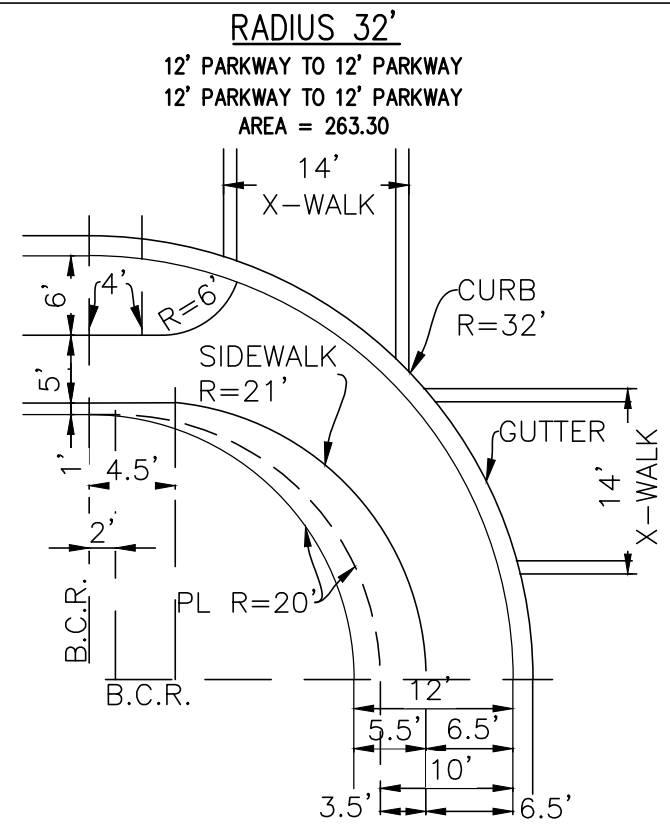
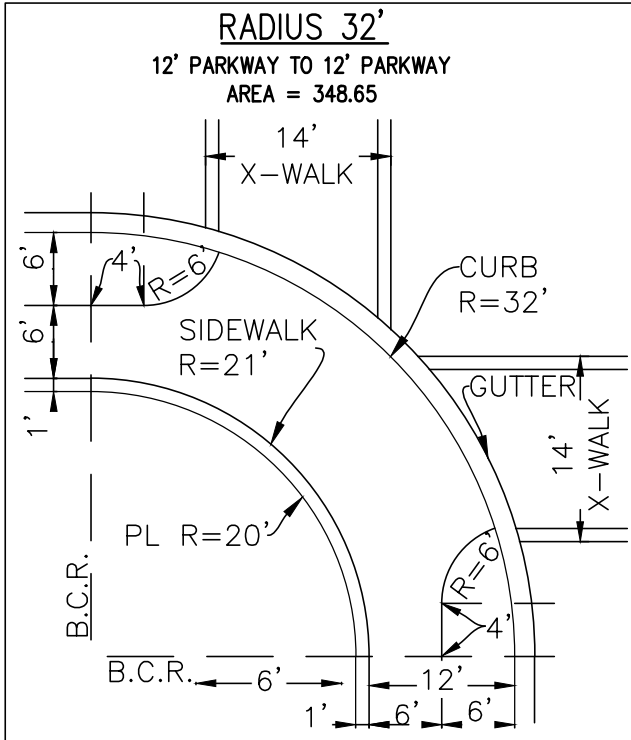


**SIDEWALK TRANSITION DETAIL**

**NOTES:**

1. ALL WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH LATEST ADOPTED STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
2. CONCRETE SHALL BE CLASS 520-C-2500.
3. MISSING SIDEWALK & SIDEWALKS REPAIRS SHALL BE PLACED TO MATCH ADJACENT SIDEWALK WIDTH & FINISH.
4. A LIGHT BROOM FINISH SHALL BE APPLIED PARALLEL TO SCORE LINES.
5. COMPACT SOIL BENEATH SIDEWALK TO 90% (MIN) RELATIVE COMPACTION PER APWA SPECIFICATIONS. PROVIDE SOIL AMENDMENT IF REQUIRED BY CITY ENGINEER.
6. MAXIMUM RUNNING SLOPE SHALL NOT EXCEED SLOPE OF ADJACENT STREET.

|                                           |      |                   |              |
|-------------------------------------------|------|-------------------|--------------|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |      |                   |              |
| <b>SIDEWALK TRANSITION DETAIL</b>         |      |                   |              |
|                                           |      | DRAWN BY: _____   | STANDARD     |
|                                           |      | CHECKED BY: _____ |              |
| △                                         | DATE | INITIAL           |              |
|                                           |      |                   | 1<br>OF<br>1 |

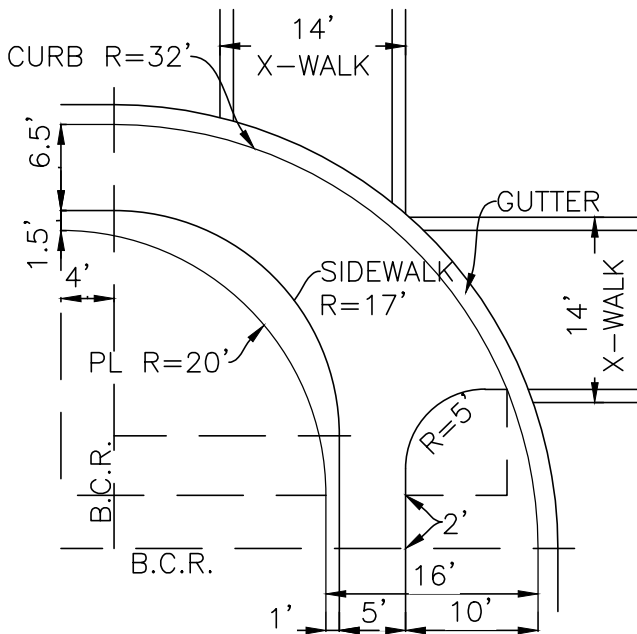


|                                                                    |          |    |
|--------------------------------------------------------------------|----------|----|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT                          |          |    |
| <b>SIDEWALK INTERSECTIONS<br/>27' &amp; 32' RADII CURB RETURNS</b> |          |    |
| DRAWN BY: _____                                                    | STANDARD | 1  |
| CHECKED BY: _____                                                  |          | OF |
| APPROVED BY: _____                                                 |          | 2  |
| DATE                                                               | INITIAL  |    |

|   |  |  |  |  |  |
|---|--|--|--|--|--|
| △ |  |  |  |  |  |
|---|--|--|--|--|--|

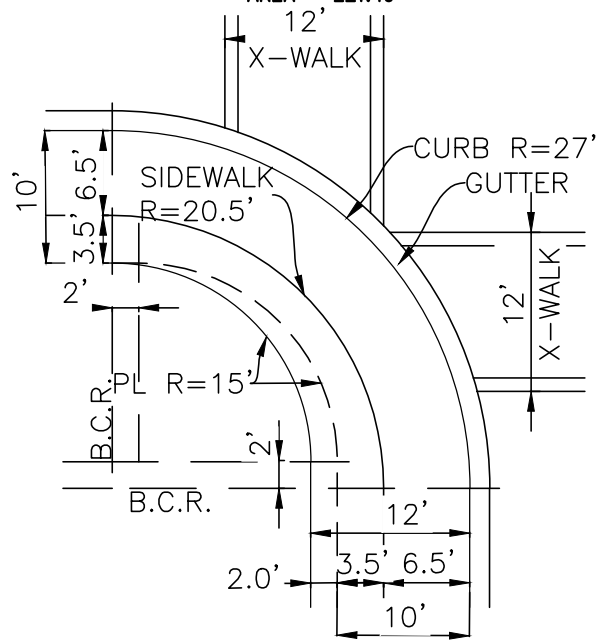
**RADIUS 32'**

8' PARKWAY TO 16' PARKWAY  
 AREA = 331.94



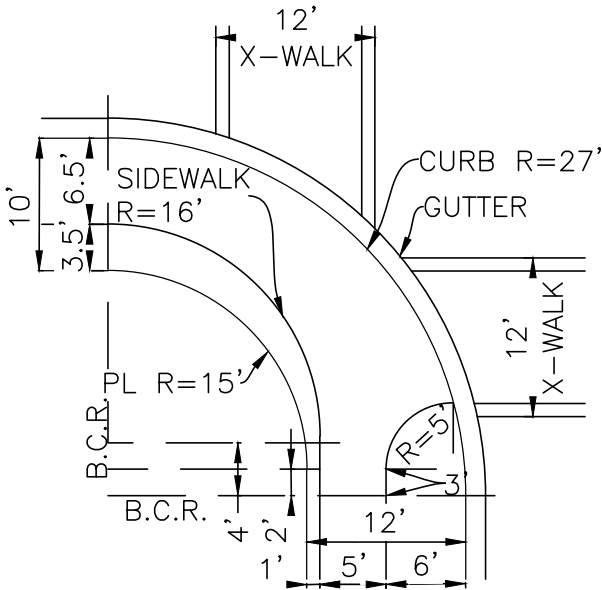
**RADIUS 27'**

10' PARKWAY TO 12' PARKWAY  
 10' PARKWAY TO 10' PARKWAY  
 AREA = 221.49



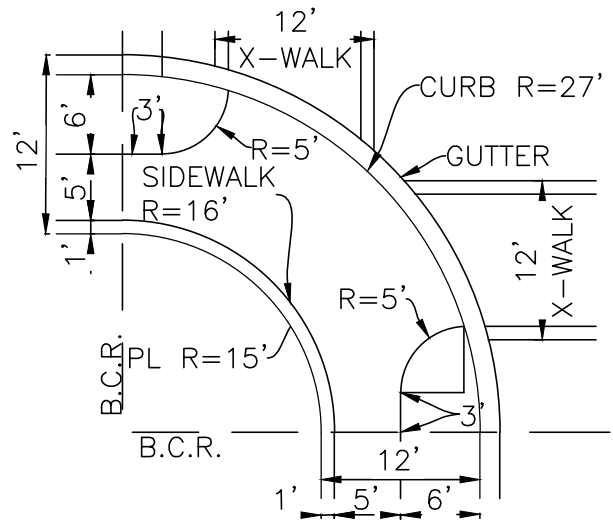
**RADIUS 27'**

10' PARKWAY TO 12' PARKWAY  
 AREA = 243.10



**RADIUS 27'**

12' PARKWAY TO 12' PARKWAY  
 AREA = 279.71



CITY OF POMONA  
 PUBLIC WORKS DEPARTMENT

**SIDEWALK INTERSECTIONS  
 27' & 32' RADII CURB RETURNS**

|   |  |      |         |
|---|--|------|---------|
|   |  |      |         |
|   |  |      |         |
|   |  |      |         |
| △ |  | DATE | INITIAL |

|              |          |    |
|--------------|----------|----|
| DRAWN BY:    | STANDARD | 2  |
| CHECKED BY:  |          | OF |
| APPROVED BY: |          | 2  |

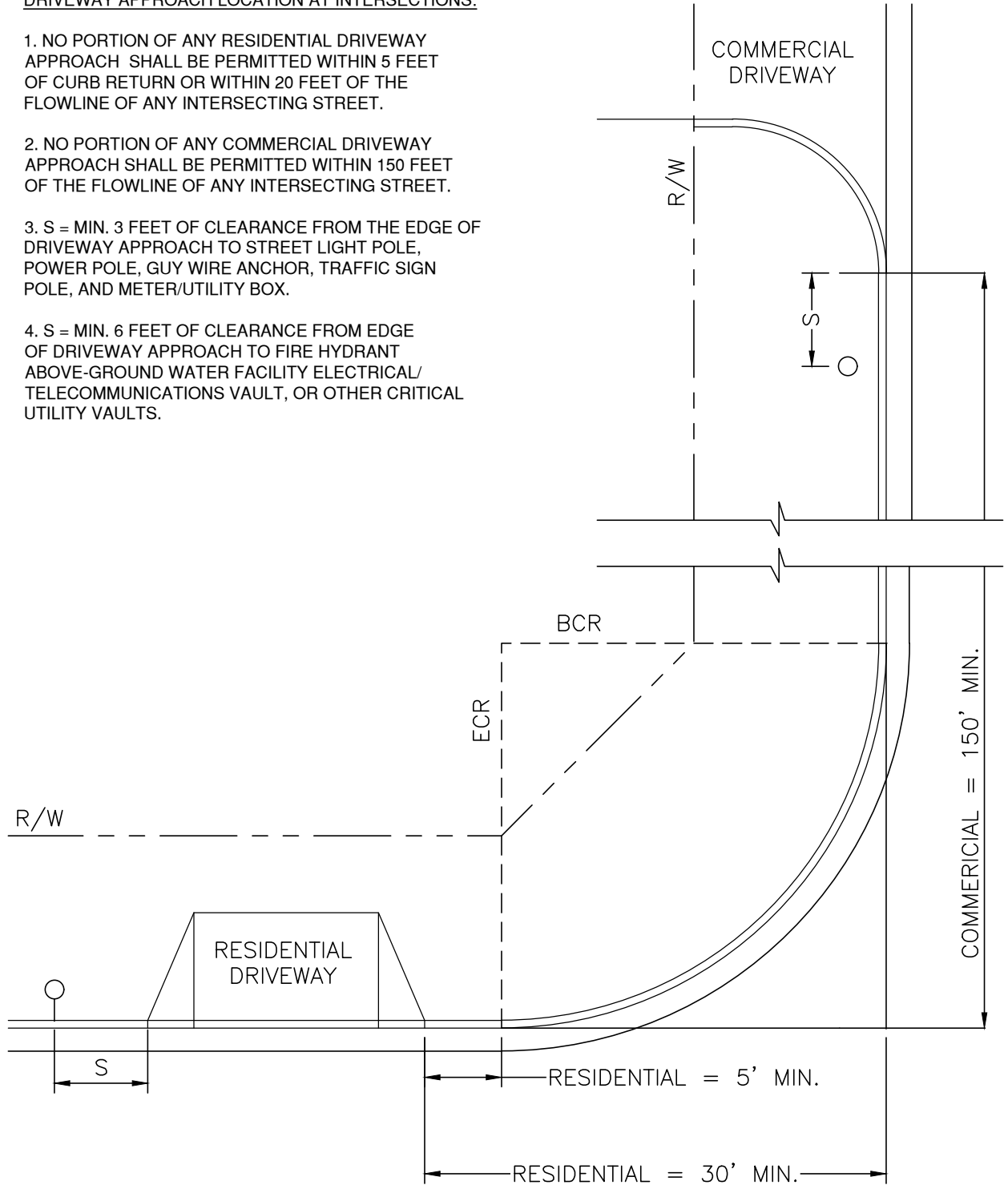
## GENERAL NOTES FOR ALL DRIVEWAY APPROACHES

1. DRIVEWAY APPROACH SHALL BE CLASS 520-C-2500 PCC (6" MIN. THICKNESS COMMERCIAL) AND SHALL UTILIZE CURING COMPOUND. ALL NEW PCC WORK REQUIRES TAMPING PER SSPWC "GREENBOOK"
2. DRIVEWAY APPROACH SHALL BE CONSTRUCTED ON 6" C.A.B. OR 6" C.M.B. OVER 6" SUBGRADE AT 90% RELATIVE COMPACTION
3. DRIVEWAY APPROACHES SHALL BE SCORED 1 1/2" DEEP AT THE CENTER OF THE APPROACH AND EQUALLY SPACED AT A MAXIMUM 10'-0" ON CENTER
4. WHEN ABUTTING SOIL HAS HIGH SULFATE CONTENT, SPECIAL CONSIDERATION SHALL BE CONSIDERING INCLUDING BUT NOT LIMITED TO 6 MIL PLASTIC SHEETING A MINIMUM OF 2' BEYOND EDGE OF CONCRETE (SEE SSPWC "GREENBOOK" FOR FURTHER REQUIREMENTS)
5. FOR NEW DRIVEWAY APPROACHES ADJACENT TO EXISTING PAVEMENT, A MINIMUM OF 2' OF EXISTING PAVEMENT SHALL BE REMOVED AND REPLACED FULL-DEPTH ADJACENT TO NEW PCC
6. DRIVEWAY APPROACH DESIGN SHALL PROVIDE FOR ADA ACCESSIBLE PATH OF TRAVEL (2% MAX. CROSS SLOPE, MINIMUM 4' WIDE)
7. NEW DRIVEWAY APPROACHES REQUIRE THE INSTALLATION OF ONE 2" PVC STREET LIGHT CONDUIT 2' BEHIND CURB AND SHALL EXTEND A MIN. OF 3 FEET BEYOND BCR/ECR OF DRIVEWAY APPROACH
8. MAX. DRIVEWAY SLOPE IS 10% MEASURED FROM THE 1" CURB LIP AT THE FLOWLINE TO THE GRADE BREAK AT THE FRONT OF THE ADA PATH OF TRAVEL
9. DRIVEWAY SLOPE AREA REQUIRES MEDIUM BROOM FINISH PARALLEL WITH CURB. DRIVEWAY SIDEWALK AREA REQUIRES MEDIUM BROOM FINISH PERPENDICULAR TO CURB LINE
10. DRIVEWAY SIDEWALK SHALL BE SCORED AT 5' ON CENTER WITH WEAKENED PLANE JOINTS EVERY 10'

|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>DRIVEWAY APPROACH GENERAL NOTES</b>    |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 1<br>OF<br>5 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |

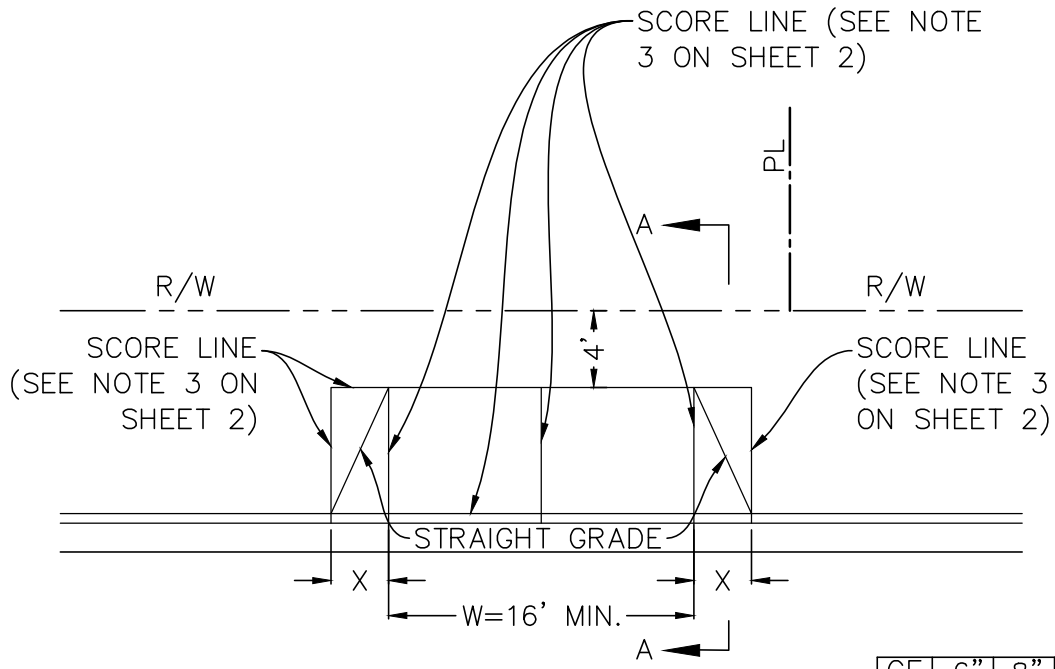
DRIVEWAY APPROACH LOCATION AT INTERSECTIONS:

1. NO PORTION OF ANY RESIDENTIAL DRIVEWAY APPROACH SHALL BE PERMITTED WITHIN 5 FEET OF CURB RETURN OR WITHIN 20 FEET OF THE FLOWLINE OF ANY INTERSECTING STREET.
2. NO PORTION OF ANY COMMERCIAL DRIVEWAY APPROACH SHALL BE PERMITTED WITHIN 150 FEET OF THE FLOWLINE OF ANY INTERSECTING STREET.
3. S = MIN. 3 FEET OF CLEARANCE FROM THE EDGE OF DRIVEWAY APPROACH TO STREET LIGHT POLE, POWER POLE, GUY WIRE ANCHOR, TRAFFIC SIGN POLE, AND METER/UTILITY BOX.
4. S = MIN. 6 FEET OF CLEARANCE FROM EDGE OF DRIVEWAY APPROACH TO FIRE HYDRANT ABOVE-GROUND WATER FACILITY ELECTRICAL/ TELECOMMUNICATIONS VAULT, OR OTHER CRITICAL UTILITY VAULTS.



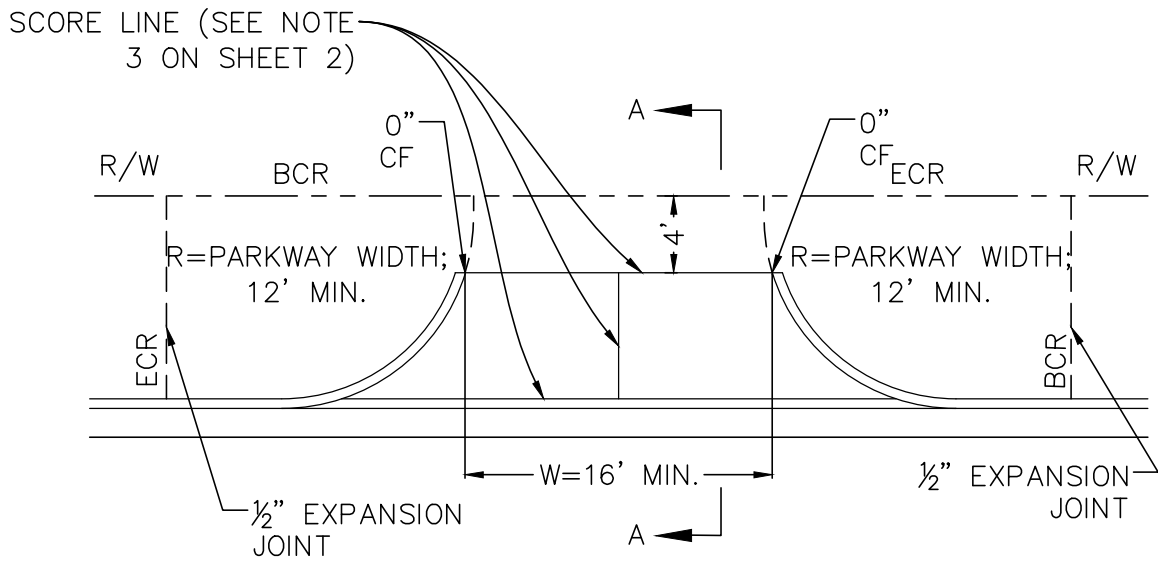
|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>DRIVEWAY APPROACH</b>                  |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 2<br>OF<br>5 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |





|    |    |    |     |
|----|----|----|-----|
| CF | 6" | 8" | >8" |
| X  | 3' | 4' | 5'  |

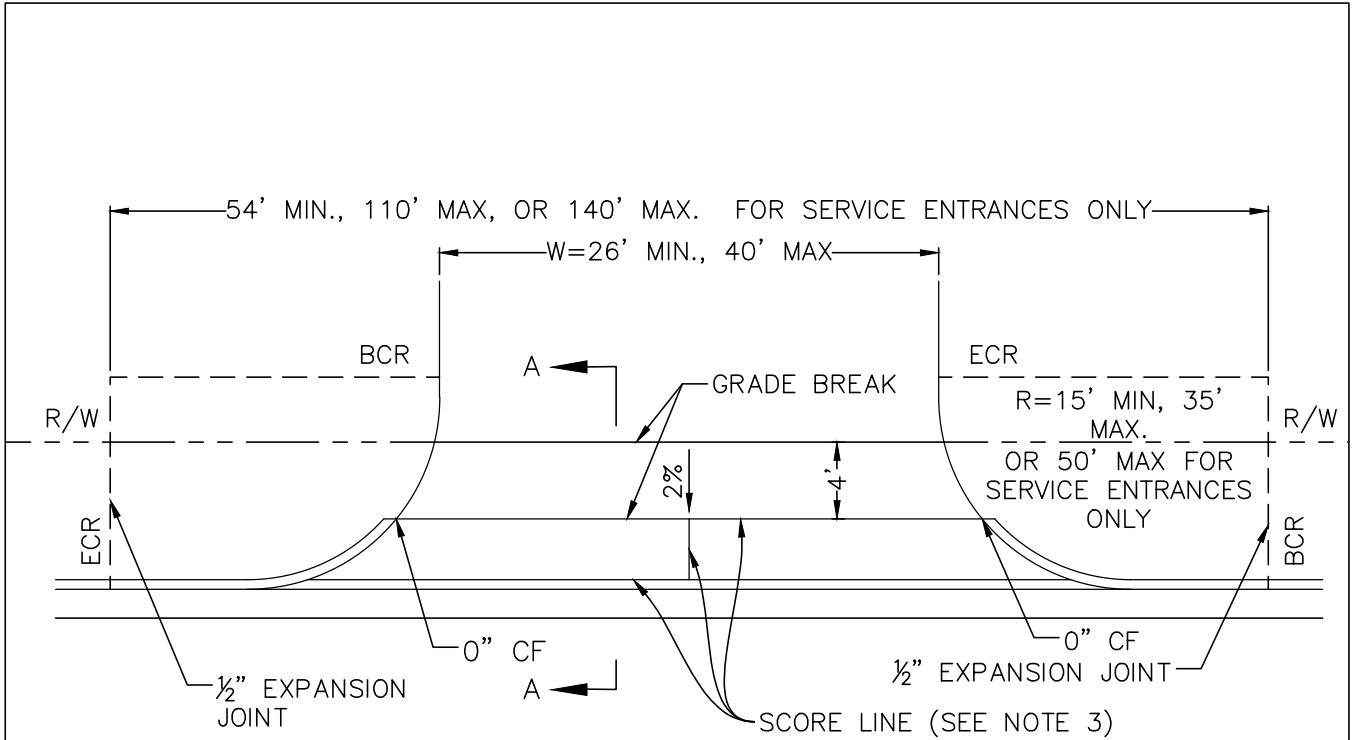
**TYPE II COMMERCIAL DRIVEWAY  
APPROACH WITH TRANSITION CURB**



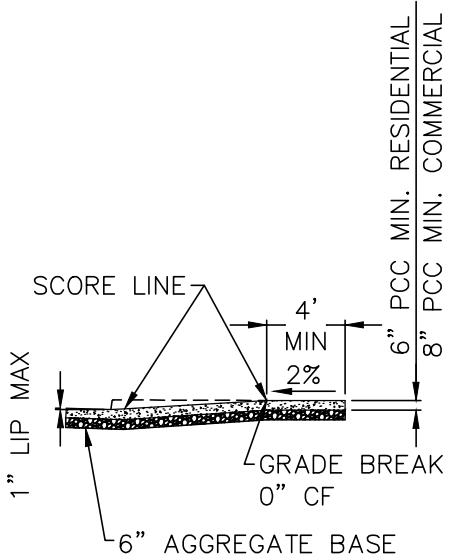
**PLAN VIEW  
TYPE III COMMERCIAL DRIVEWAY  
APPROACH WITH SMALL CURB RADIUS**

SEE SHEET 2 FOR GENERAL NOTES FOR DRIVEWAY APPROACHES AND SECTION A-A

|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>COMMERCIAL DRIVEWAY APPROACH</b>       |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 3<br>OF<br>5 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |

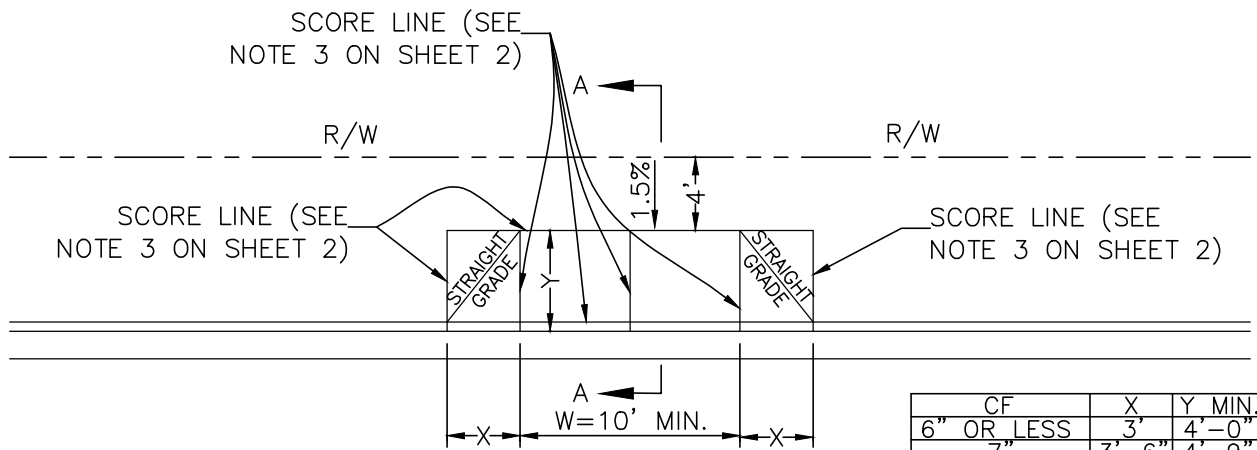


**PLAN VIEW**  
**TYPE I COMMERCIAL DRIVEWAY**  
**APPROACH WITH LARGE**  
**RADIUS CURBS**



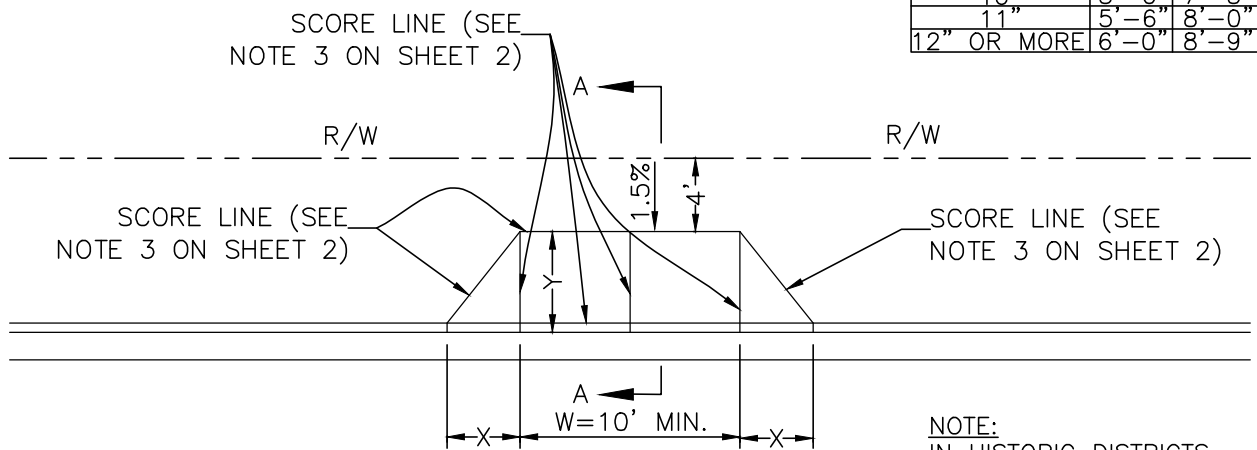
**SECTION A-A**  
**NOT TO SCALE**

|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>COMMERCIAL DRIVEWAY APPROACH</b>       |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 4<br>OF<br>5 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |



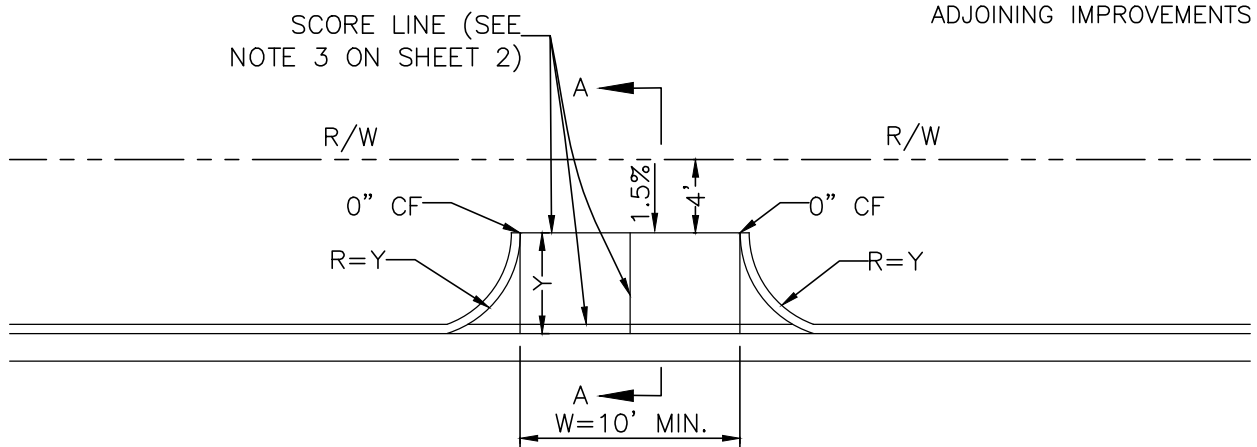
**PLAN VIEW – TYPE A RESIDENTIAL DRIVEWAY APPROACH**

| CF          | X     | Y MIN. |
|-------------|-------|--------|
| 6" OR LESS  | 3'    | 4'-0"  |
| 7"          | 3'-6" | 4'-9"  |
| 8"          | 4'-0" | 5'-8"  |
| 9"          | 4'-6" | 6'-6"  |
| 10"         | 5'-0" | 7'-3"  |
| 11"         | 5'-6" | 8'-0"  |
| 12" OR MORE | 6'-0" | 8'-9"  |



**PLAN VIEW – TYPE B RESIDENTIAL DRIVEWAY APPROACH**

**NOTE:**  
IN HISTORIC DISTRICTS,  
PCC SCORE PATTERN  
AND FINISH SHALL MATCH  
ADJOINING IMPROVEMENTS



**PLAN VIEW – TYPE C RESIDENTIAL DRIVEWAY APPROACH**

**SEE SHEET 2 FOR GENERAL NOTES FOR DRIVEWAY APPROACHES AND SECTION A-A**

CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**RESIDENTIAL DRIVEWAY APPROACH**

|   |                         |      |         |                    |          |              |
|---|-------------------------|------|---------|--------------------|----------|--------------|
|   |                         |      |         | DRAWN BY: _____    | STANDARD | 5<br>OF<br>5 |
|   |                         |      |         | CHECKED BY: _____  |          |              |
| △ | UPDATED ENTIRE STANDARD |      |         | APPROVED BY: _____ |          |              |
| △ |                         | DATE | INITIAL |                    |          |              |

## GENERAL NOTES FOR CURB RAMPS

1. CONCRETE SHALL BE CLASS 310-C-17 (520-C-2500) AND SHALL BE 4" THICK.
2. ALL RAMPS SHALL BE POURED MONOLITHICALLY.
3. TWO CURB RAMPS (DUAL) ARE REQUIRED AT EACH SIDEWALK CORNER. IN ALTERATIONS WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT TWO CURB RAMPS FROM BEING INSTALLED AT A STREET CORNER, A SINGLE CURB RAMP IS PERMITTED. EACH CURB RAMP SHALL CONNECT THE PEDESTRIAN ACCESS ROUTE AT EACH PEDESTRIAN STREET CROSSING.
4. PEDESTRIAN ACCESS ROUTES SHALL BE FIRM, STABLE, AND SLIP RESISTANT. PROVIDE SLIP RESISTANT TEXTURE ON SIDEWALKS AND CURB RAMP BY BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP AND OR PERPENDICULAR TO THE PEDESTRIAN TRAVEL. EXTEND TEXTURE THE FULL WIDTH AND LENGTH OF THE CURB RAMP INCLUDING SIDE FLARES. DO NOT SCORE OR MAKE GROOVES IN SLOPED SURFACE.
5. CHANGES IN LEVEL OF 0.25" HIGH MAXIMUM SHALL BE PERMITTED TO BE VERTICAL AND WITHOUT EDGE TREATMENT. CHANGES IN LEVEL BETWEEN 0.25" AND 0.5" HIGH MAXIMUM SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2.
6. OPPOSING CURB RAMPS AT A SINGLE CROSSING SHALL LINE UP. ALIGN THE CURB RAMP WITH THE CROSSWALK SO THERE IS A STRAIGHT PATH OF TRAVEL FROM THE TOP OF THE RAMP TO THE CURB RAMP ON THE OTHER SIDE, TO THE MAXIMUM EXTENT FEASIBLE.
7. PULL BOXES, MANHOLES, VAULTS, AND OTHER UTILITIES SHALL BE RELOCATED OR INCORPORATED INTO THE CURB RAMP AREA PROVIDED THAT THE ACCESS COVER IS STABLE, FIRM, SLIP RESISTANT, AND FLUSH OR ADJUSTED TO GRADE. COORDINATE THE WORK WITH THE ENGINEER.
8. UTILITY POLES MAY BE INCORPORATED INTO THE FLARES OF THE CURB RAMP PROVIDED THAT THE REQUIRED ACCESSIBLE ROUTE WIDTH IS COMPLIANT.
9. THE RUNNING SLOPE OF THE RAMP SHALL NOT EXCEED 8.33%. IF THE CONDITION OF THE STREET AND SIDEWALK IS SUCH THAT THE EXISTING SLOPES DO NOT ALLOW THE INSTALLATION OF THE REQUIRED RUNNING SLOPE, THEN THE RAMP LENGTH SHALL BE EXTENDED 15 LINEAR FEET MAXIMUM EVEN IF THE REQUIRED SLOPE IS NOT ACHIEVED.
10. THE SLOPE OF THE FLARES WITHIN THE PEDESTRIAN CIRCULATION ROUTE SHALL BE 10% MAXIMUM. IF THE CONDITION OF THE STREET AND SIDEWALK IS SUCH THAT THE EXISTING SLOPES DO NOT ALLOW THE INSTALLATION OF THE REQUIRED SLOPE, THEN THE FLARE LENGTH SHALL BE EXTENDED 10 LINEAR FEET MAXIMUM.
11. USE THE FOLLOWING CURB RAMP TYPES PER IDENTIFIED RIGHT OF WAY LIMITS IN THE TABLE BELOW:

| <u>RIGHT OF WAY (ROW)<br/>LIMITS</u> | <u>CURB RAMP<br/>TYPE</u> | <u>NOTES</u>                                                                                                                                                              |
|--------------------------------------|---------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 10' OR MORE                          | A1 TO A5                  | MEASURED FROM FACE OF CURB TO PROPERTY LINE.                                                                                                                              |
| 8' TO 10'                            | B                         | SHALL BE USED AT RESTRICTED ROW LIMITS TO ACCOMMODATE MULTI-DIRECTIONAL ACCESS IF IT IS TECHNICALLY INFEASIBLE TO USE A STANDARD RAMP OR TWO DIRECTIONAL CURB RAMPS.      |
| LESS THAN 8'                         | F, G, H, I                | SHALL BE USED AT RESTRICTED ROW LIMITS TO ACCOMMODATE MULTI-DIRECTIONAL ACCESS IF IT IS TECHNICALLY INFEASIBLE TO USE A STANDARD CURB RAMP OR TWO DIRECTIONAL CURB RAMPS. |
| ANY                                  | J, K                      | SHALL BE USED AT ALLEY CORNERS.                                                                                                                                           |

|   |      |         |                    |                                           |          |               |
|---|------|---------|--------------------|-------------------------------------------|----------|---------------|
|   |      |         |                    | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |               |
|   |      |         |                    | <b>CURB RAMP NOTES 1 OF 2</b>             |          |               |
|   |      |         |                    | DRAWN BY: _____                           | STANDARD | 1<br>OF<br>16 |
|   |      |         |                    | CHECKED BY: _____                         |          |               |
| △ | DATE | INITIAL | APPROVED BY: _____ |                                           |          |               |

**GENERAL NOTES FOR CURB RAMPS (CONTINUED)**

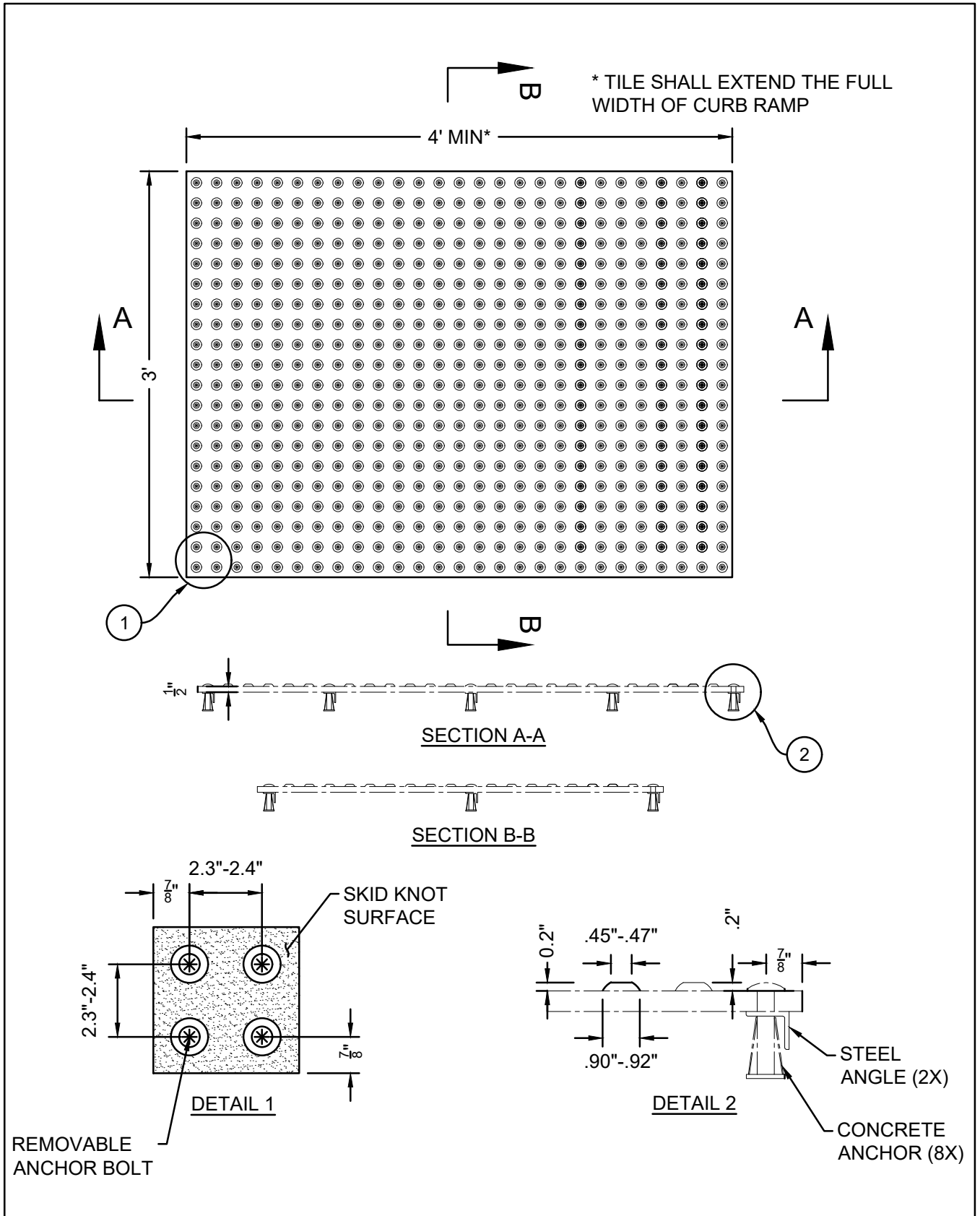
12. GRADE BREAKS AT THE TOP AND BOTTOM OF THE RAMPS AND CURB RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
13. PONDING IS NOT ALLOWED WITHIN THE CURB RAMP LIMITS.
14. TRANSITIONS FROM RAMPS TO WALKS AND SIDEWALK GUTTER OR STREET SURFACE SHALL BE FLUSH AND FREE FROM ABRUPT CHANGES.
15. DIAGONAL OR CORNER TYPE CURB RAMPS WITH RETURNED CURBS OR OTHER WELL-DEFINED EDGES SHALL HAVE THE EDGES PARALLEL TO THE DIRECTION OF PEDESTRIAN FLOW. DIAGONAL CURB RAMPS WITH FLARED SIDES SHALL HAVE A SEGMENT OF CURB 2' LONG MINIMUM LOCATED ON EACH SIDE OF THE CURB RAMPS AND WITHIN THE MARKED CROSSING.
16. DIAGONAL CURB RAMPS SHALL HAVE A CLEAR 4' X 4' MINIMUM TURNING SPACE BEYOND THE BOTTOM GRADE BREAK WITHIN THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE ACTIVE TRAFFIC LANES OF THE ROADWAY (VEHICULAR AND BIKE LANES)
17. CURB RAMPS AND FORM WORK SLOPES SHALL BE CHECKED WITH A 2' LONG DIGITAL LEVEL. NO PORTION OF A RAMP RUN SHALL EXCEED THE MAXIMUM SLOPE REQUIREMENT.
18. REVISE GUTTERS AT CURB RAMPS TO EXTEND 4' FROM BACK OF CURB.
19. THE COUNTER SLOPE WITHIN 4' OF THE GUTTER SHALL BE 5% MAXIMUM. IN ALTERATIONS IF THE COUNTER SLOPE OF 5% MAXIMUM CANNOT BE ACHIEVED, THEN ADJUST THE SLOPE OR ELEVATION OF THE RAMP SO THE COMBINED COUNTER SLOPE AND RAMP SLOPES DO NOT EXCEED 13.33%.
20. THE SLOPE OF THE RAMP SHALL BE UNIFORM ALONG EACH RAMP RUN.
21. THE CROSS SLOPE OF THE RAMP SHALL BE MEASURED PERPENDICULARLY TO THE PATH OR DIRECTION OF TRAVEL.
22. PEDESTRIAN PATHWAY SHALL NOT EXCEED 2% CROSS SLOPE. 1.5% IS RECOMMENDED DESIGN SLOPE.
23. ANY DEVIATIONS FROM THESE PROVISIONS REQUIRES PRIOR APPROVAL FROM THE ENGINEER.
24. EXISTING CONCRETE SPANDREL OF A CROSS GUTTER SHALL BE REMOVED AND REPLACED IN ITS ENTIRETY AS PART OF THE CURB RAMP INSTALLATION.
25. EXISTING CONCRETE ALLEY APRON SHALL BE REMOVED AND REPLACED IN ITS ENTIRETY AS PART OF CURB RAMP INSTALLATION.
26. CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED CARS.

|   |  |      |         |                                           |          |               |
|---|--|------|---------|-------------------------------------------|----------|---------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |               |
|   |  |      |         | <b>CURB RAMP NOTES 2 OF 2</b>             |          |               |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 2<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |          |               |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |               |

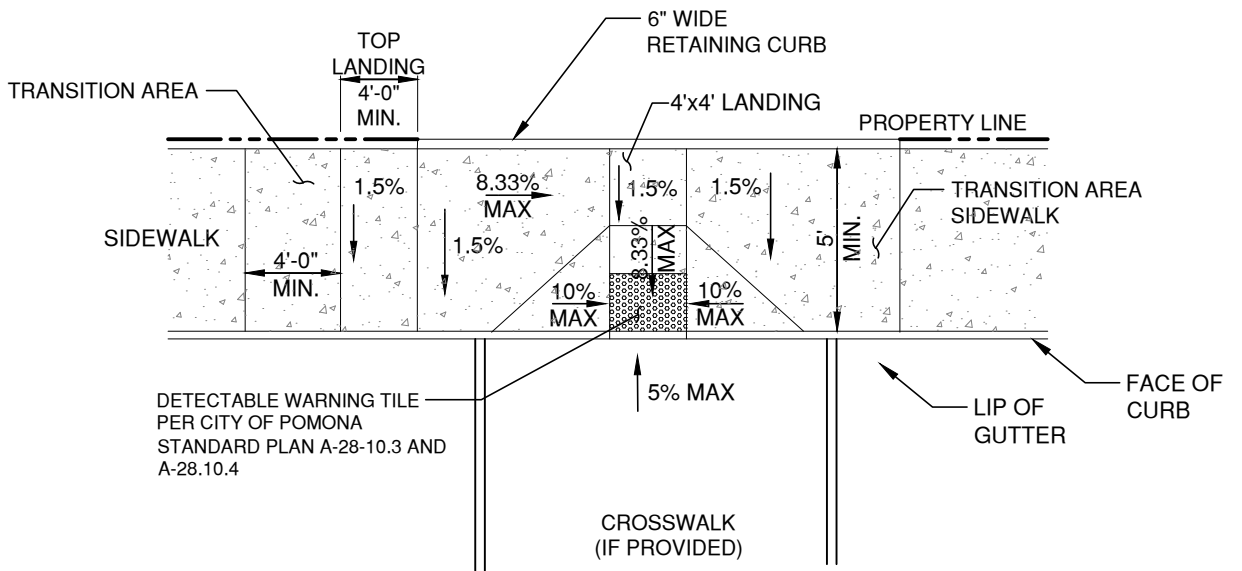
## GENERAL NOTES FOR DETECTABLE WARNINGS

1. CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSISTS OF RAISED TRUNCATED DOMES.
2. THE DETECTABLE WARNING TILE SHALL BE SLIP RESISTANT AND SHALL CONSIST OF AN INLINE PATTERN OF RAISED TRUNCATED DOMES. THE TILE SHALL BE PLACED WHERE THE DOMES ARE IN LINE WITH THE DIRECTION OF TRAVEL.
3. DETECTABLE WARNING TILES AT ALL 0" CURBS SHALL BE YELLOW CONFORMING TO FEDERAL STANDARDS 595C TABLE IV, COLOR NO. 33538 .
4. DETECTABLE WARNING TILES AT CURB RAMPS SHALL BE PER CITY STANDARD DETAIL A-28-10.4
5. DETECTABLE WARNING TILE PRODUCTS SHALL BE PER THE CITY'S APPROVED MATERIALS LIST.
6. NO VOIDS SHALL BE ALLOWED BELOW THE DETECTABLE WARNING TILES.
7. DETECTABLE WARNING TILES SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB IS 6" MINIMUM AND 8" MAXIMUM FROM THE LINE AT THE FACE OF THE CURB. EXCEPTIONS IN WHICH DETECTABLE WARNING TILES ARE NOT REQUIRED ARE WHEN BOTH ENDS OF THE BOTTOM OF THE GRADE BREAK ARE LESS THAN 5' FROM BACK OF CURB AND ACCESS ACROSS THE RETAINING CURB IS PREVENTED BY LANDSCAPE OR PROTECTIVE RAILING.

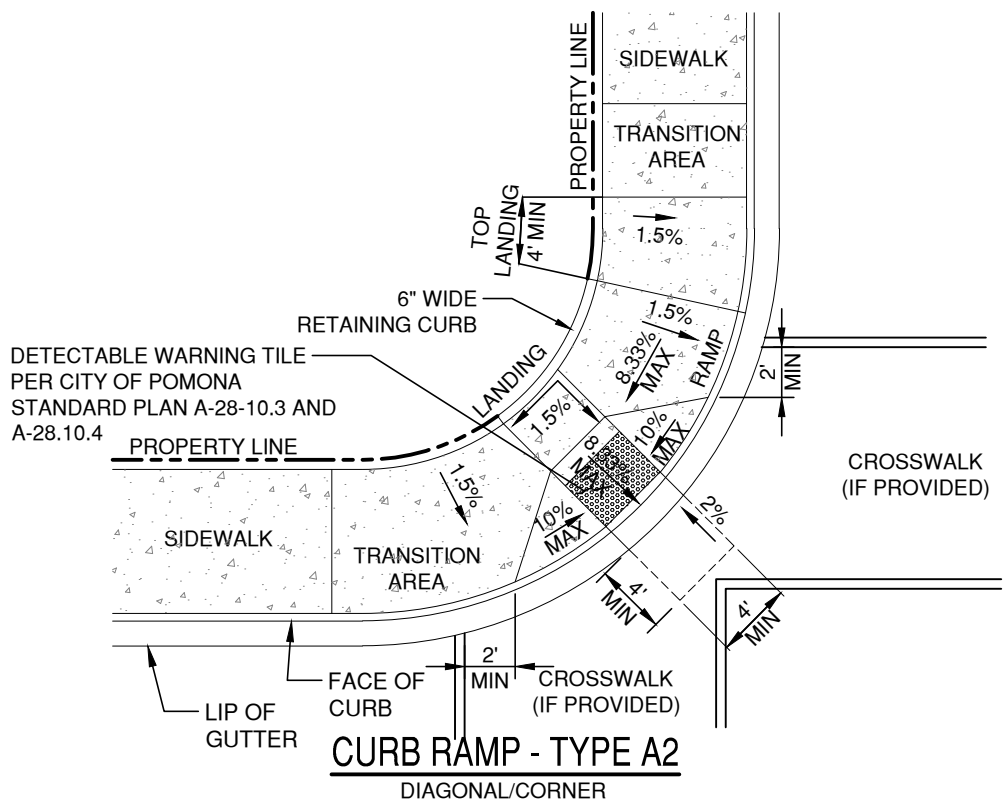
|   |  |      |         |                                           |          |               |
|---|--|------|---------|-------------------------------------------|----------|---------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |               |
|   |  |      |         | <b>DETECTABLE WARNINGS NOTES</b>          |          |               |
|   |  |      |         | DRAWN BY:                                 | STANDARD | 3<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |          |               |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |               |



|   |  |      |         |                                           |          |               |
|---|--|------|---------|-------------------------------------------|----------|---------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |               |
|   |  |      |         | <b>DETECTABLE WARNINGS</b>                |          |               |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 4<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |          |               |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |               |



**CURB RAMP - TYPE A1**  
PERPENDICULAR



**CURB RAMP - TYPE A2**  
DIAGONAL/CORNER

CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

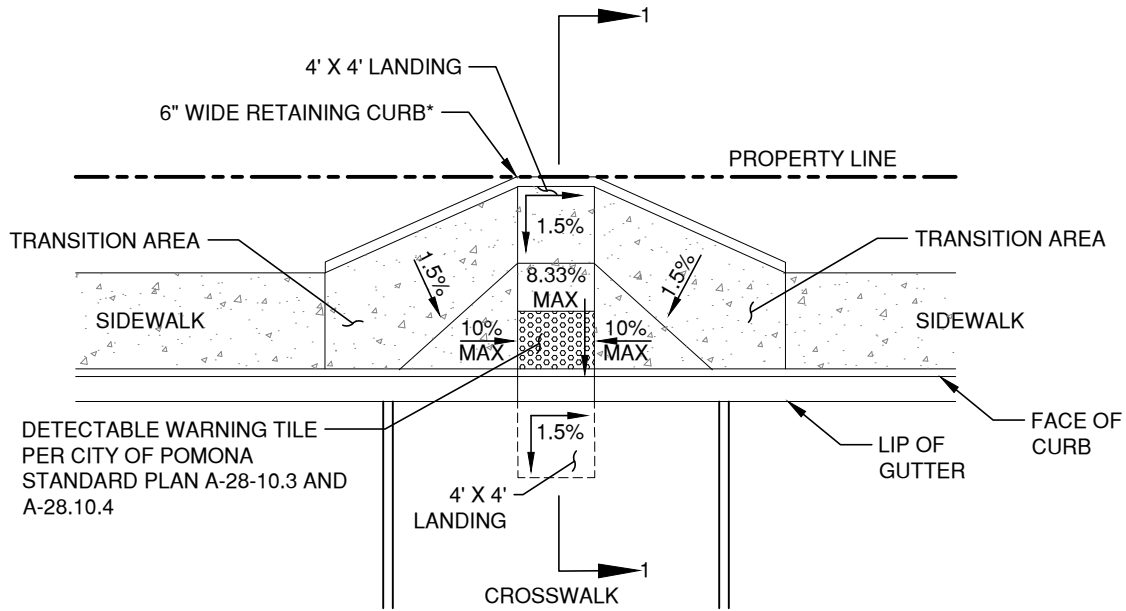
**CURB RAMP TYPE A1, A2**

|   |      |         |
|---|------|---------|
| △ | DATE | INITIAL |
|---|------|---------|

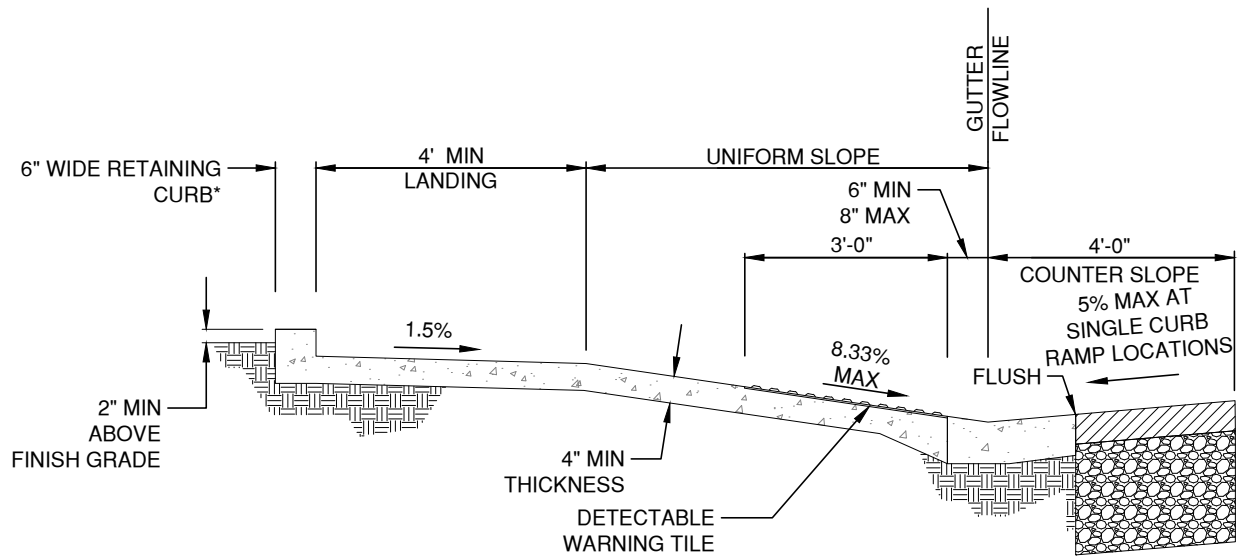
|              |  |
|--------------|--|
| DRAWN BY:    |  |
| CHECKED BY:  |  |
| APPROVED BY: |  |

|          |         |
|----------|---------|
| STANDARD | 5 OF 16 |
|----------|---------|





**CURB RAMP - TYPE A3**  
PERPENDICULAR

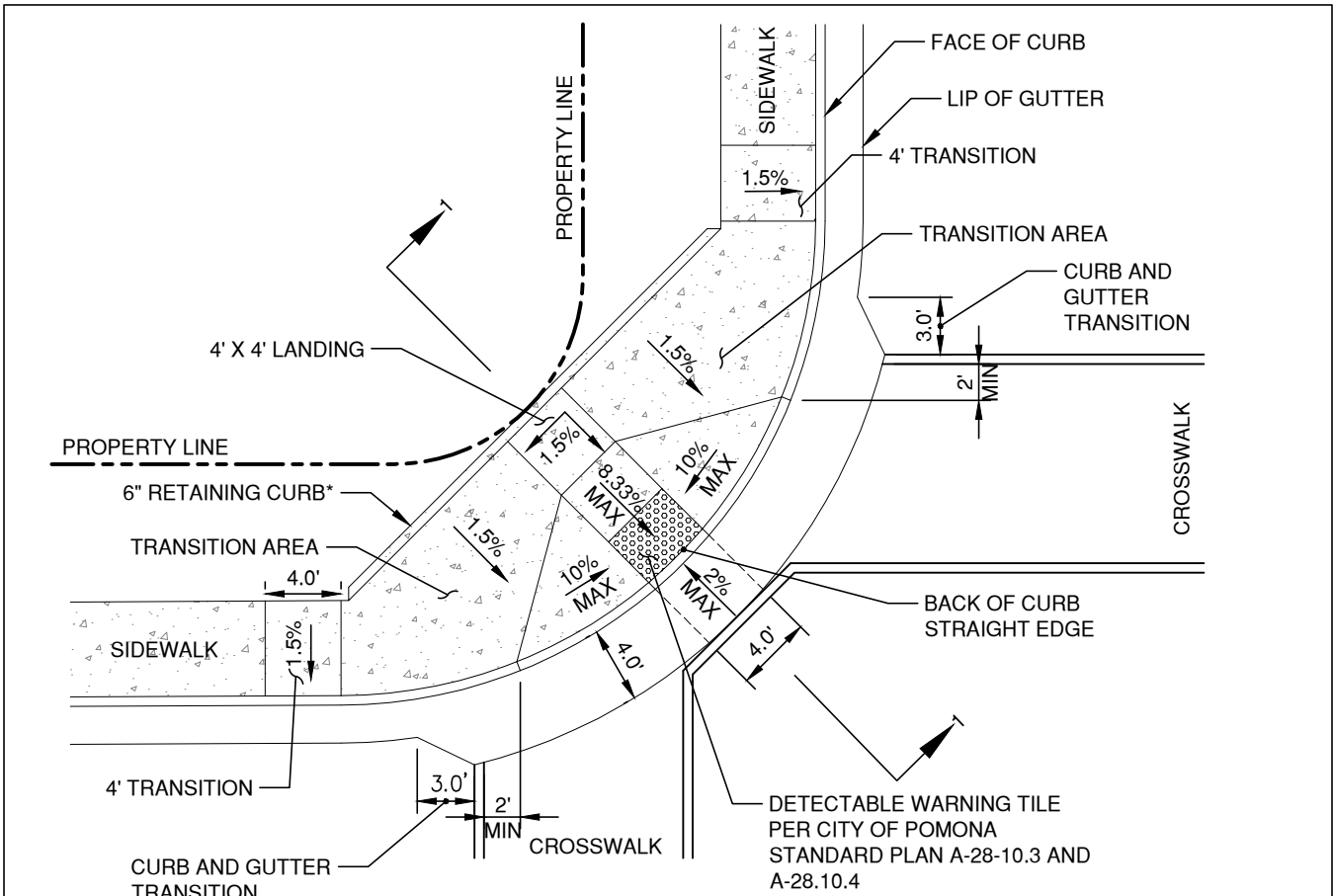


**SECTION 1-1**

**NOTES:**

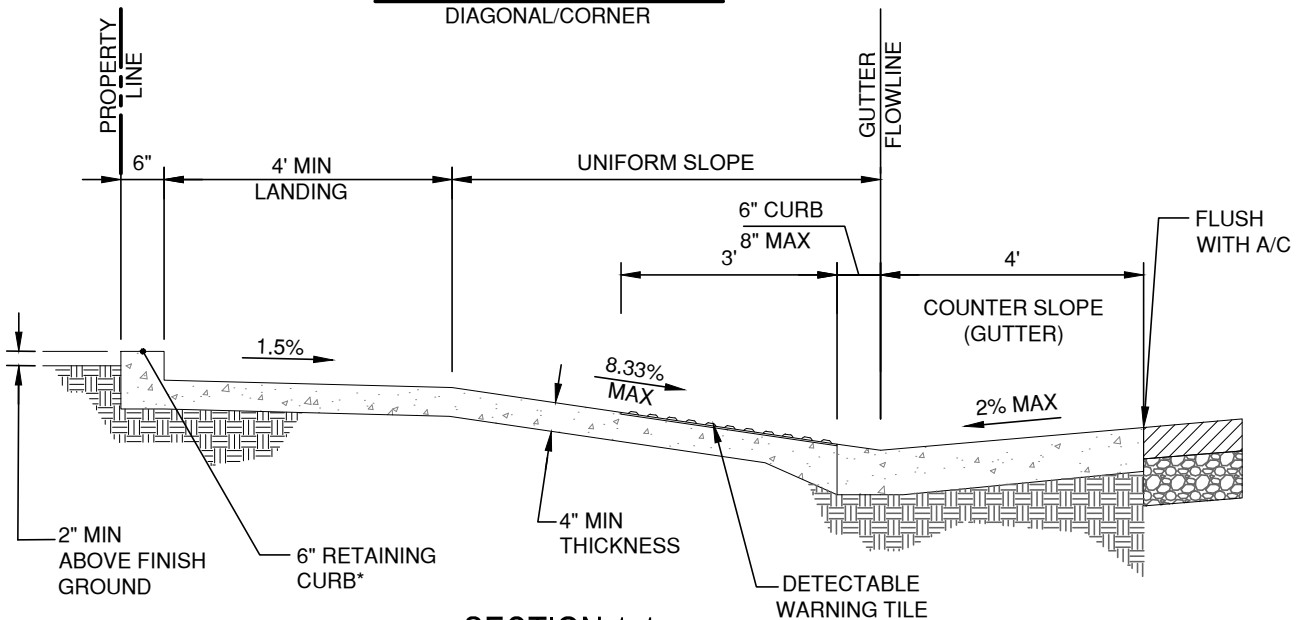
\* 6" WIDE RETAINING CURB MAY BE OMITTED IF THE GROUND SURFACE WILL BE AT THE SAME ELEVATION AS THE CURB RAMP SURFACE

|   |  |      |         |                                           |          |               |
|---|--|------|---------|-------------------------------------------|----------|---------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |               |
|   |  |      |         | <b>CURB RAMP TYPE A3</b>                  |          |               |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 6<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |          |               |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |               |



### CURB RAMP - TYPE A4

DIAGONAL/CORNER



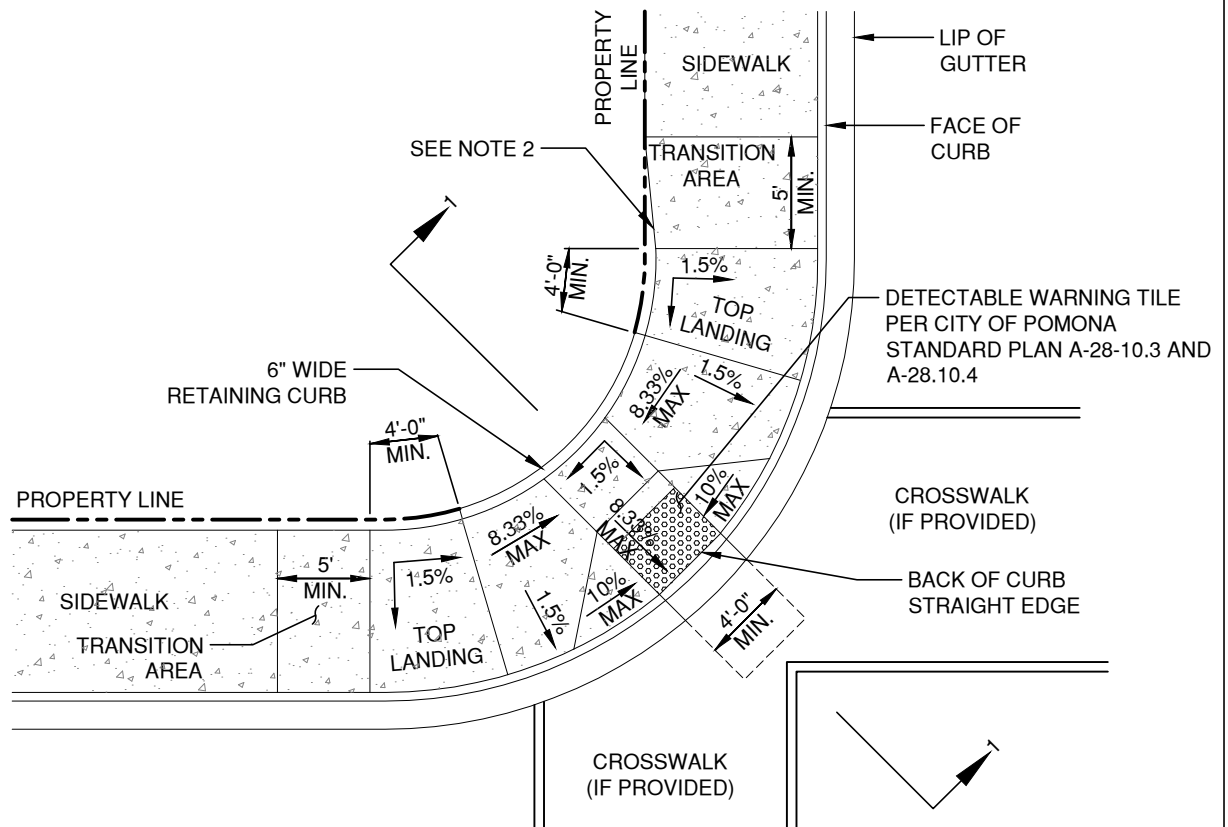
**NOTES:**

\* 6" WIDE RETAINING CURB MAY BE OMITTED IF THE GROUND SURFACE WILL BE AT THE SAME ELEVATION AS THE CURB RAMP SURFACE

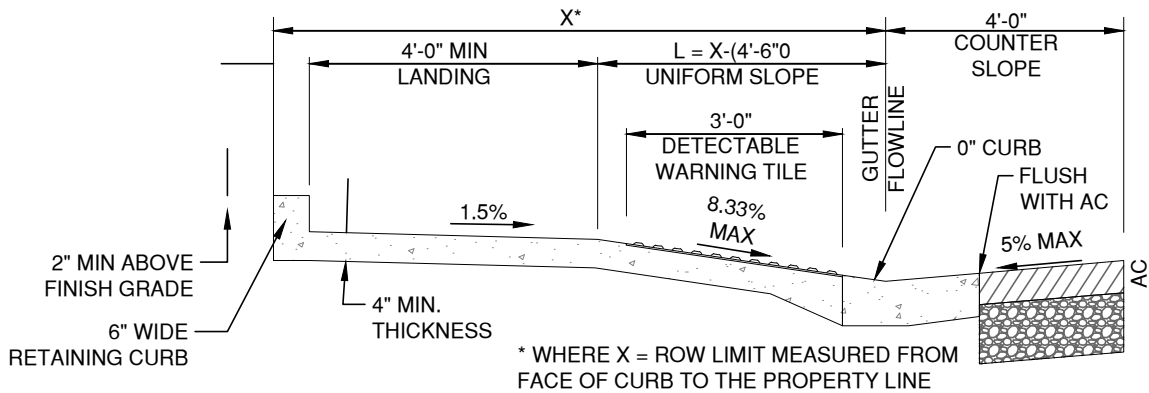
CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**CURB RAMP TYPE A4**

|   |      |         |                    |          |               |
|---|------|---------|--------------------|----------|---------------|
| △ | DATE | INITIAL | DRAWN BY: _____    | STANDARD | 7<br>OF<br>16 |
|   |      |         | CHECKED BY: _____  |          |               |
|   |      |         | APPROVED BY: _____ |          |               |



**CURB RAMP - TYPE A5**  
COMBINATION

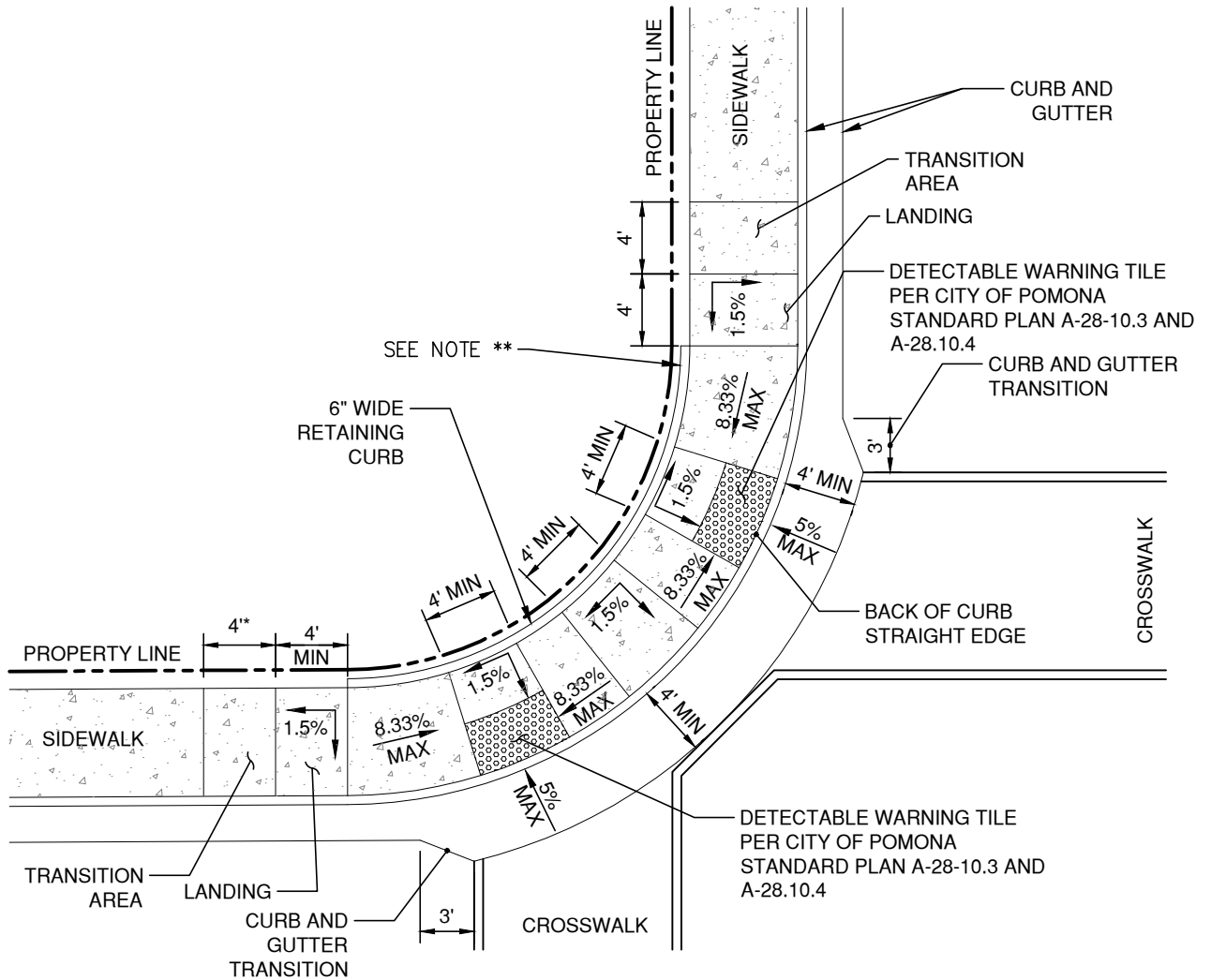


**SECTION 1-1**

**NOTES:**

1. GRADE BREAK AT THE TOP AND BOTTOM OF RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN.
2. THE BACK EDGE OF THE TOP LANDING SHALL BE ALIGNED WITH THE FRONT OF THE RETAINING CURB. THE SIDEWALK SHALL BE TAPERED IN THE TRANSITION AREA PRIOR TO THE TOP LANDING.

|   |  |      |         |                                           |          |               |
|---|--|------|---------|-------------------------------------------|----------|---------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |               |
|   |  |      |         | <b>CURB RAMP TYPE A5</b>                  |          |               |
|   |  |      |         | DRAWN BY:                                 | STANDARD | 8<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |          |               |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |               |



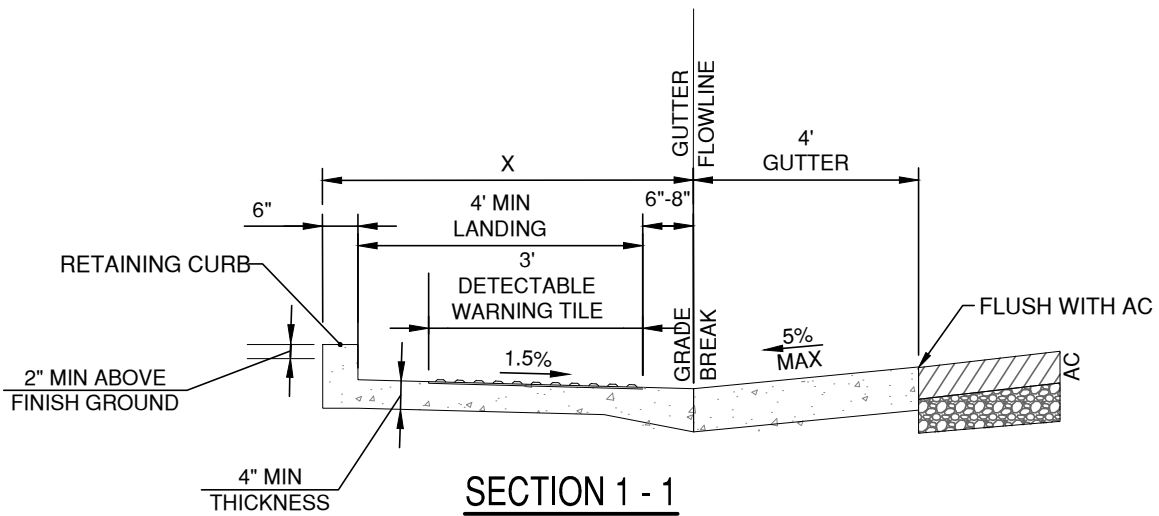
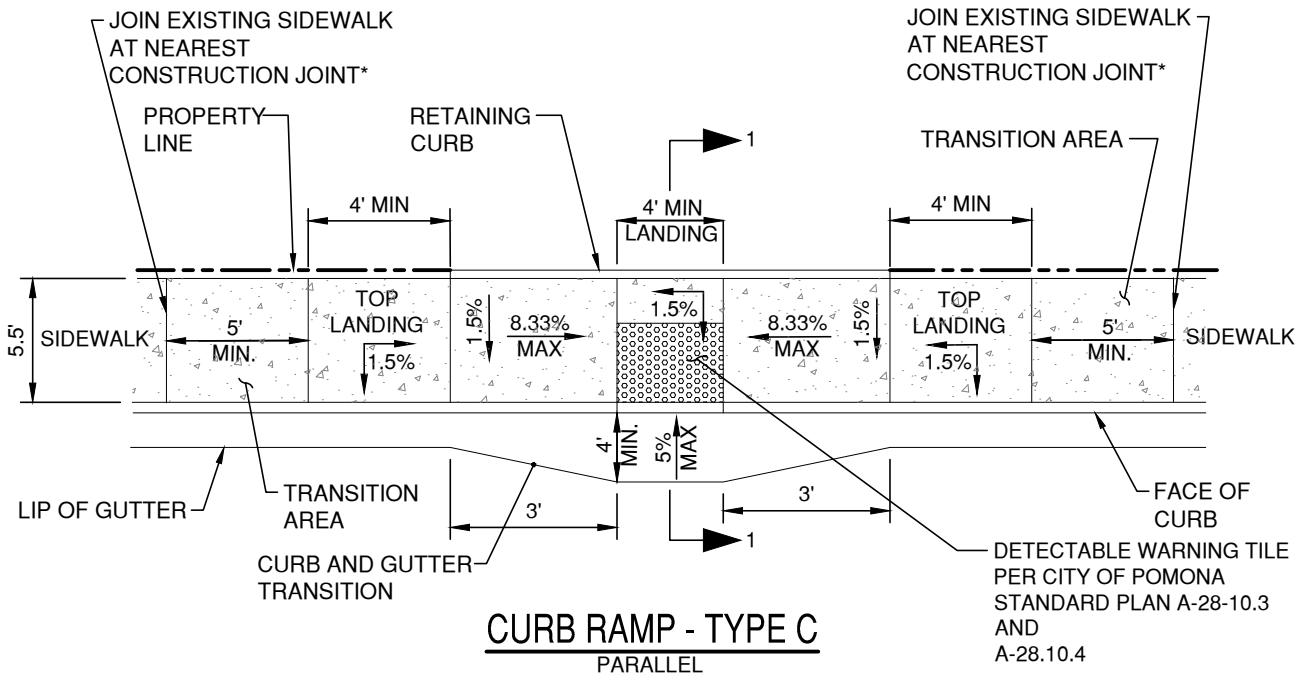
## CURB RAMP - TYPE B

DUAL PARALLEL

**NOTES:**

- \* THE CURB PORTION AT THE OPENING OF THE CURB RAMP SHALL SLOPE TOWARDS THE GUTTER FLOWLINE 5% TO PREVENT DEBRIS COLLECTED AT THE LANDING.
- \*\* THE BACK EDGE OF THE TOP LANDING SHALL BE ALIGNED WITH THE FRONT OF THE RETAINING CURB. THE SIDEWALK SHALL BE TAPERED IN THE TRANSITION AREA PRIOR TO THE TOP LANDING.

|   |  |      |         |                                           |  |          |               |
|---|--|------|---------|-------------------------------------------|--|----------|---------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |  |          |               |
|   |  |      |         | <b>CURB RAMP TYPE B</b>                   |  |          |               |
|   |  |      |         | DRAWN BY: _____                           |  | STANDARD | 9<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |  |          |               |
| △ |  |      |         | APPROVED BY: _____                        |  |          |               |
|   |  | DATE | INITIAL |                                           |  |          |               |

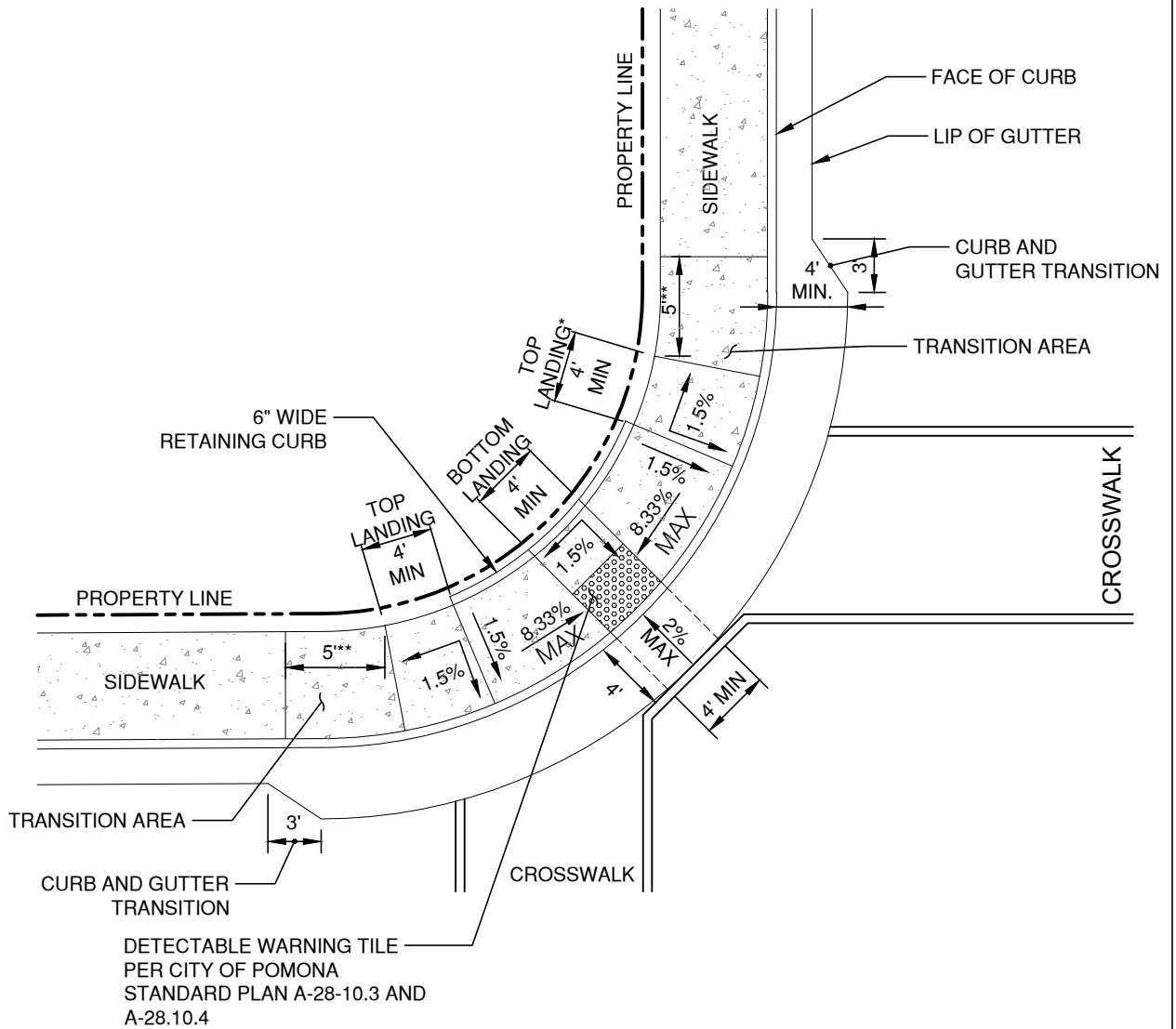


\* WHERE X = RIGHT OF WAY LIMIT MEASURED FROM FACE OF CURB TO THE PROPERTY LINE

**NOTES:**

- \* WHEN TRANSITIONING FROM NEW CONSTRUCTION TO EXISTING SURFACES, VERIFY CROSS SLOPES DO NOT CREATE A SAFETY, OR TRIPPING HAZARD. TRANSITION DISTANCE IS A RECOMMENDED MINIMUM. TO BE REVIEWED ON A CASE BY CASE BASIS.

|   |  |      |         |                                           |                |
|---|--|------|---------|-------------------------------------------|----------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |                |
|   |  |      |         | <b>CURB RAMP TYPE C</b>                   |                |
|   |  |      |         | DRAWN BY:                                 | STANDARD       |
|   |  |      |         | CHECKED BY: _____                         |                |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |                |
|   |  |      |         |                                           | 10<br>OF<br>16 |



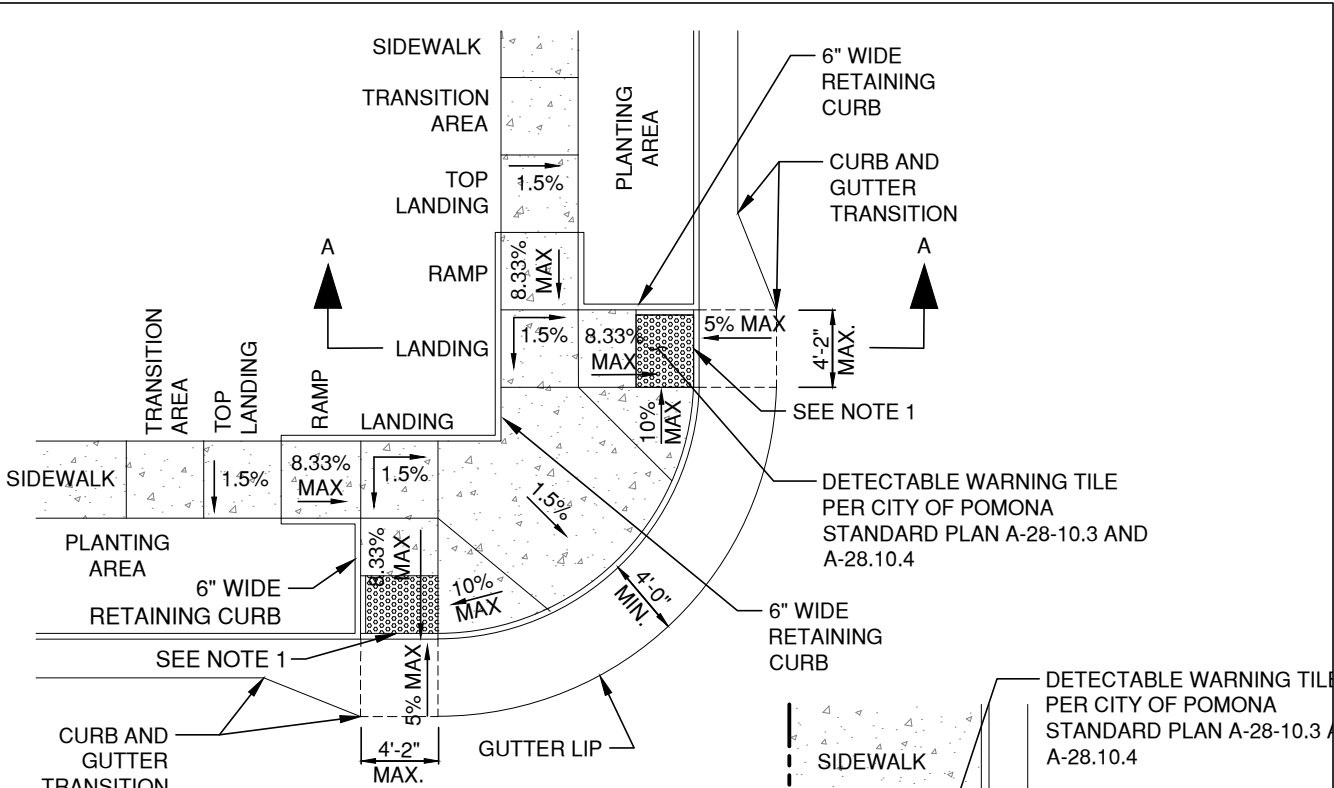
**CURB RAMP - TYPE D**  
PARALLEL

**NOTES:**

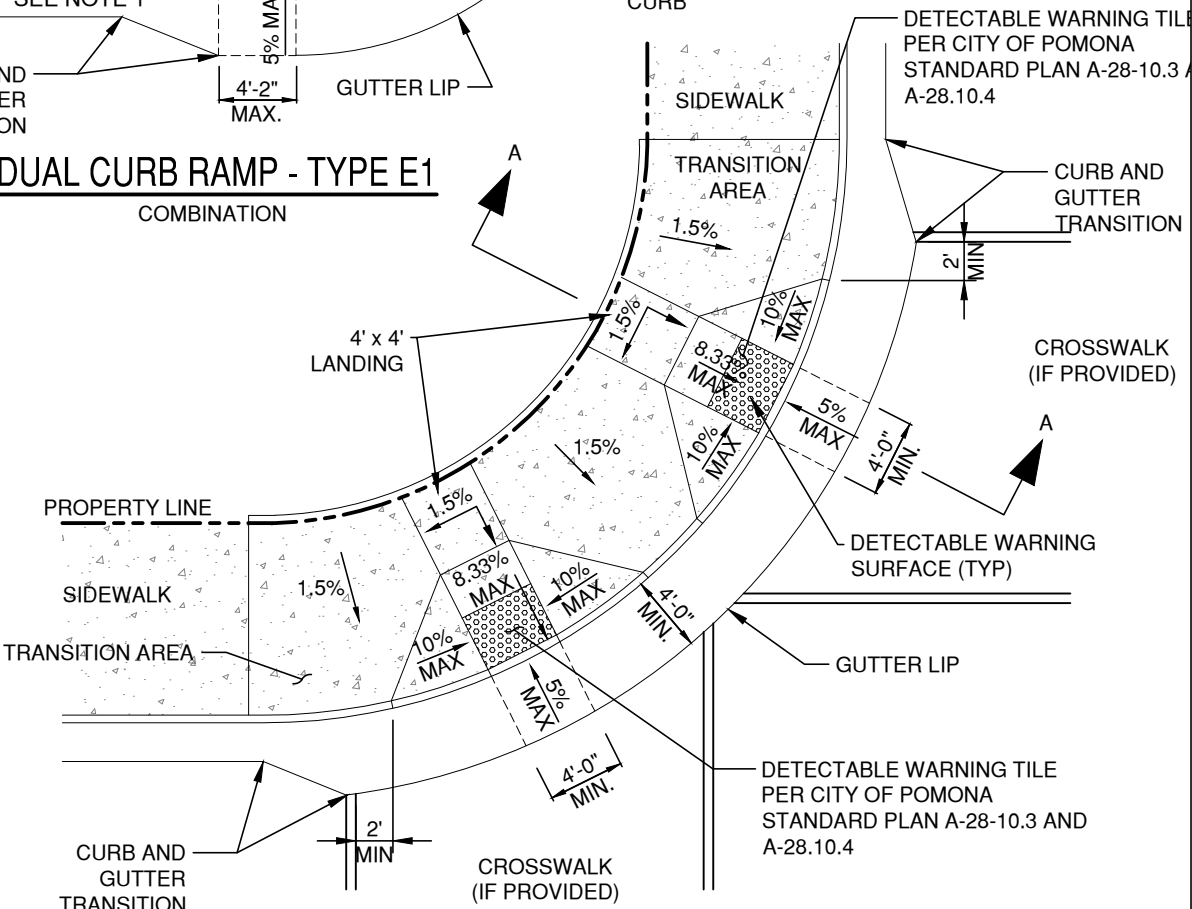
\* THE BACK EDGE OF THE TOP LANDING SHALL BE ALIGNED WITH THE FRONT OF THE RETAINING CURB. THE SIDEWALK SHALL BE TAPERED IN THE TRANSITION AREA PRIOR TO THE TOP LANDING.

\*\* WHEN TRANSITIONING FROM NEW CONSTRUCTION TO EXISTING SURFACES, VERIFY CROSS SLOPES DO NOT CREATE A SAFETY, OR TRIPPING HAZARD. TRANSITION DISTANCE IS A RECOMMENDED MINIMUM. TO BE REVIEWED ON A CASE BY CASE BASIS.

|   |  |      |         |                                           |          |                |
|---|--|------|---------|-------------------------------------------|----------|----------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |                |
|   |  |      |         | <b>CURB RAMP TYPE D</b>                   |          |                |
|   |  |      |         | DRAWN BY:                                 | STANDARD | 11<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____                         |          |                |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |                |

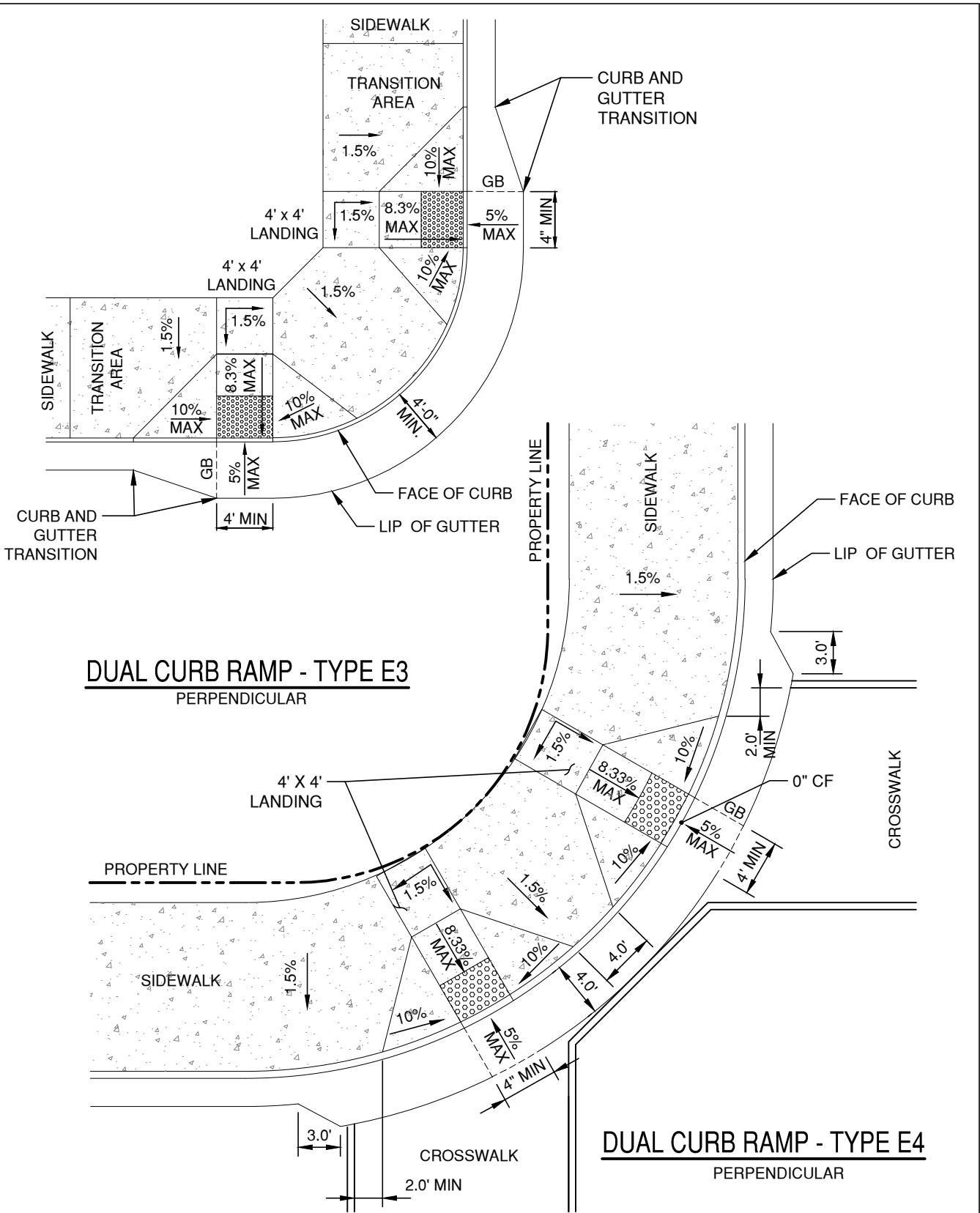


**DUAL CURB RAMP - TYPE E1**  
COMBINATION



**DUAL CURB RAMP - TYPE E2**  
PERPENDICULAR

|                                           |      |                    |          |                |
|-------------------------------------------|------|--------------------|----------|----------------|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |      |                    |          |                |
| <b>CURB RAMP TYPE E1, E2</b>              |      |                    |          |                |
|                                           |      | DRAWN BY:          | STANDARD | 12<br>OF<br>16 |
|                                           |      | CHECKED BY: _____  |          |                |
|                                           |      | APPROVED BY: _____ |          |                |
| △                                         | DATE | INITIAL            |          |                |

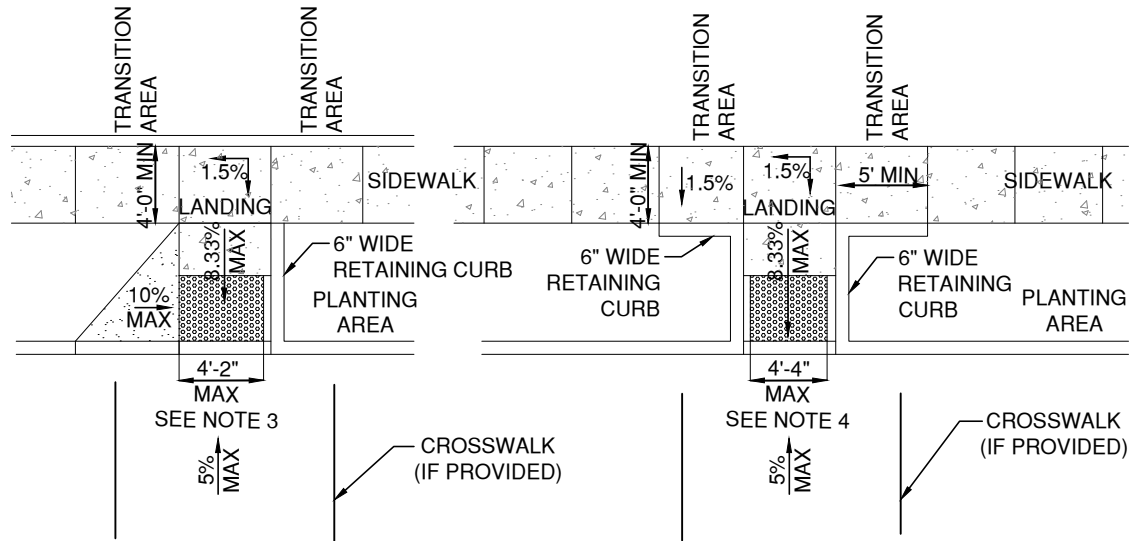


CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**CURB RAMP TYPE E3, E4**

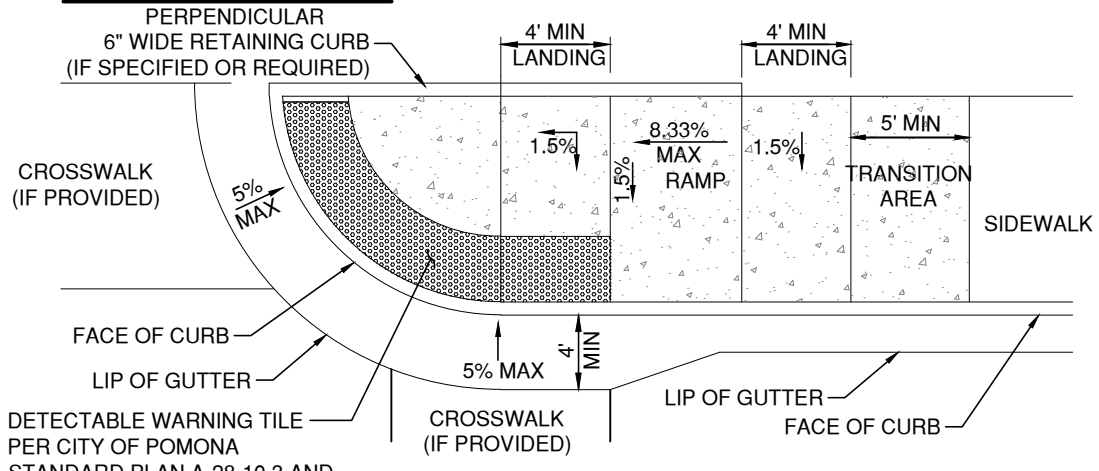
|   |      |         |                    |          |                |
|---|------|---------|--------------------|----------|----------------|
| △ | DATE | INITIAL | DRAWN BY: _____    | STANDARD | 13<br>OF<br>16 |
|   |      |         | CHECKED BY: _____  |          |                |
|   |      |         | APPROVED BY: _____ |          |                |





**CURB RAMP - TYPE F**

**CURB RAMP - TYPE G**  
PERPENDICULAR



DETECTABLE WARNING TILE  
PER CITY OF POMONA  
STANDARD PLAN A-28-10.3 AND  
A-28.10.4

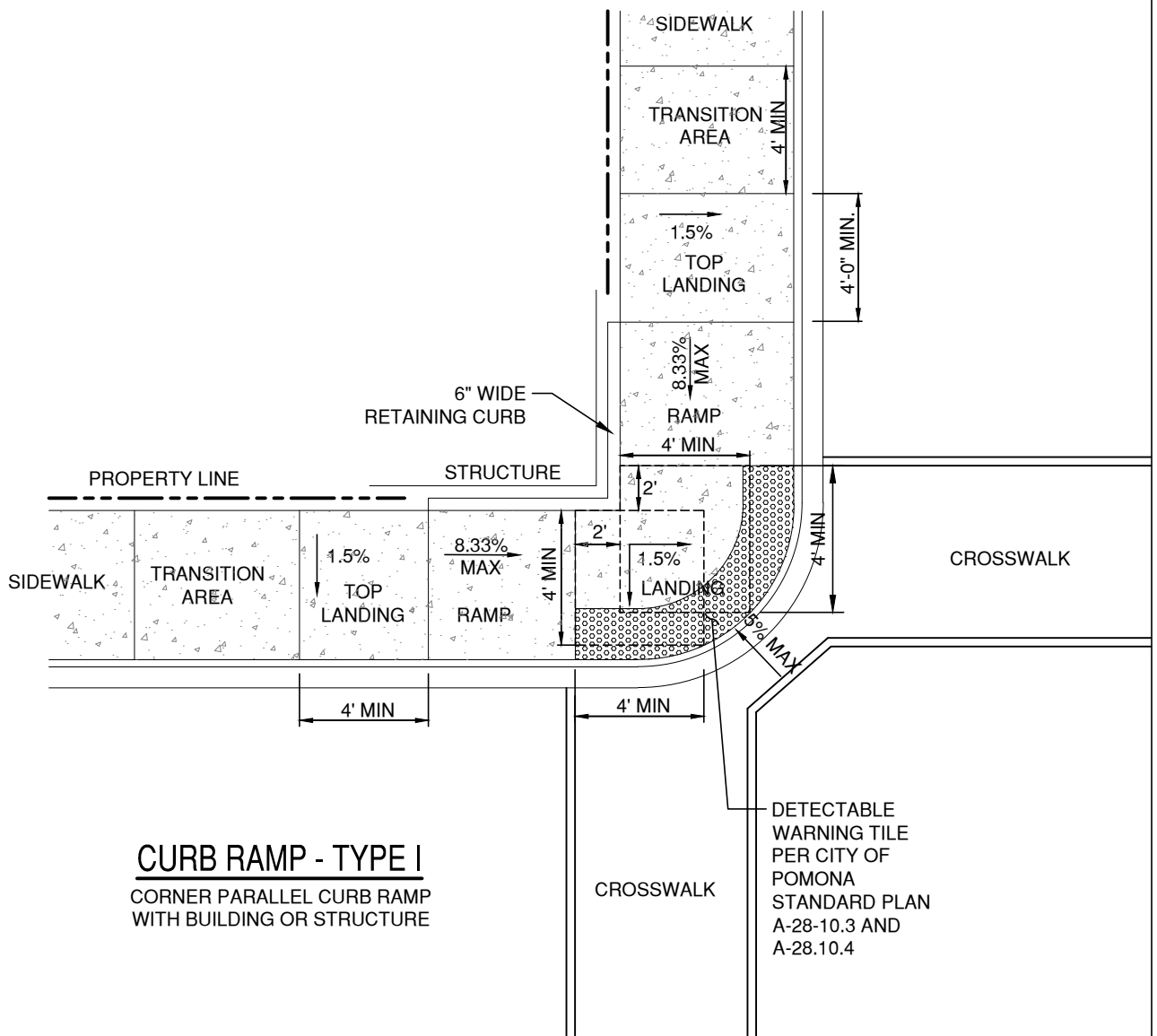
**NOTES:**

1. CASE F THROUGH H MAY ONLY BE USED IF THERE ARE SITE CONSTRAINTS.
2. TRANSITION AREA REQUIRED IF EXISTING SIDEWALK HAS A CROSS SLOPE THAT EXCEEDS 2%.
3. CASE F CURB RAMP - 4'-0" WIDE DETECTABLE WARNING TILE. IF A FLARE CANNOT BE INSTALLED, WIDEN CURB RAMP WIDTH TO 4'-2" MAXIMUM TO ACCOMMODATE FORMWORK FOR THE 6" WIDE RETAINING CURB.
4. CASE G CURB RAMP - 4'-0" WIDE DETECTABLE WARNING TILE. WIDEN CURB RAMP WIDTH TO 4'-4" MAXIMUM TO ACCOMMODATE FORMWORK FOR TWO 6" WIDE RETAINING CURBS.

CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**CURB RAMP TYPE F, G, H**

|   |      |         |                    |          |                |
|---|------|---------|--------------------|----------|----------------|
| △ | DATE | INITIAL | DRAWN BY:          | STANDARD | 14<br>OF<br>16 |
|   |      |         | CHECKED BY: _____  |          |                |
|   |      |         | APPROVED BY: _____ |          |                |

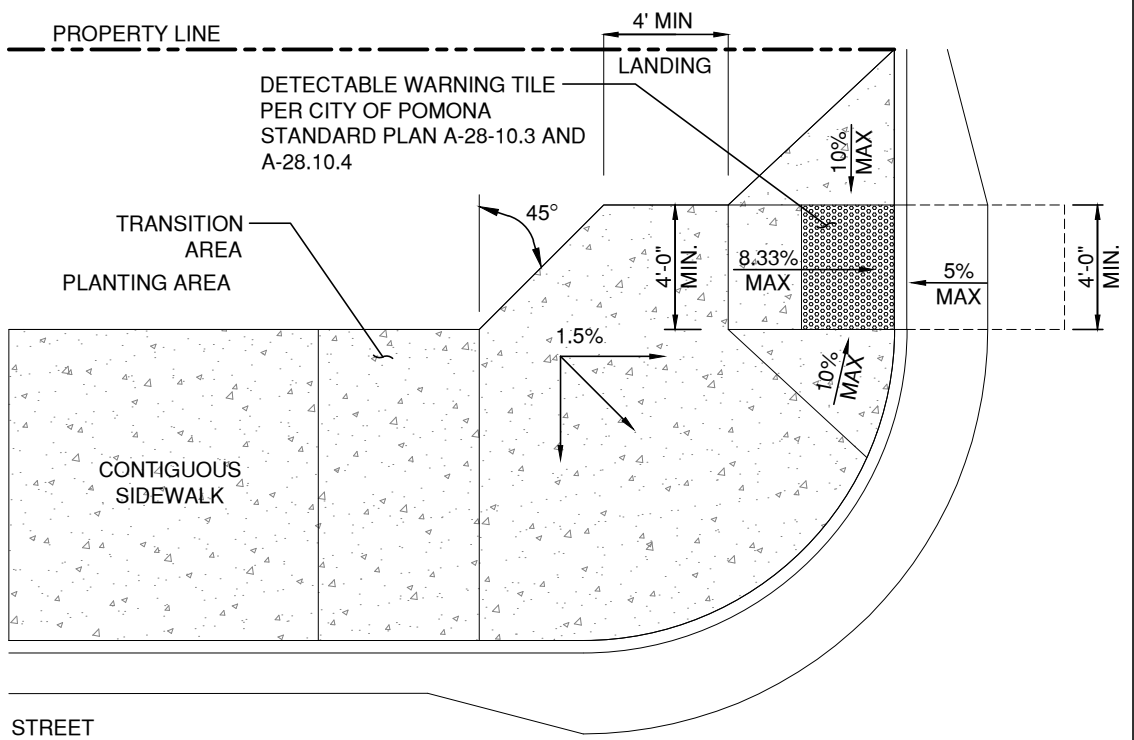


**CURB RAMP - TYPE I**  
 CORNER PARALLEL CURB RAMP  
 WITH BUILDING OR STRUCTURE

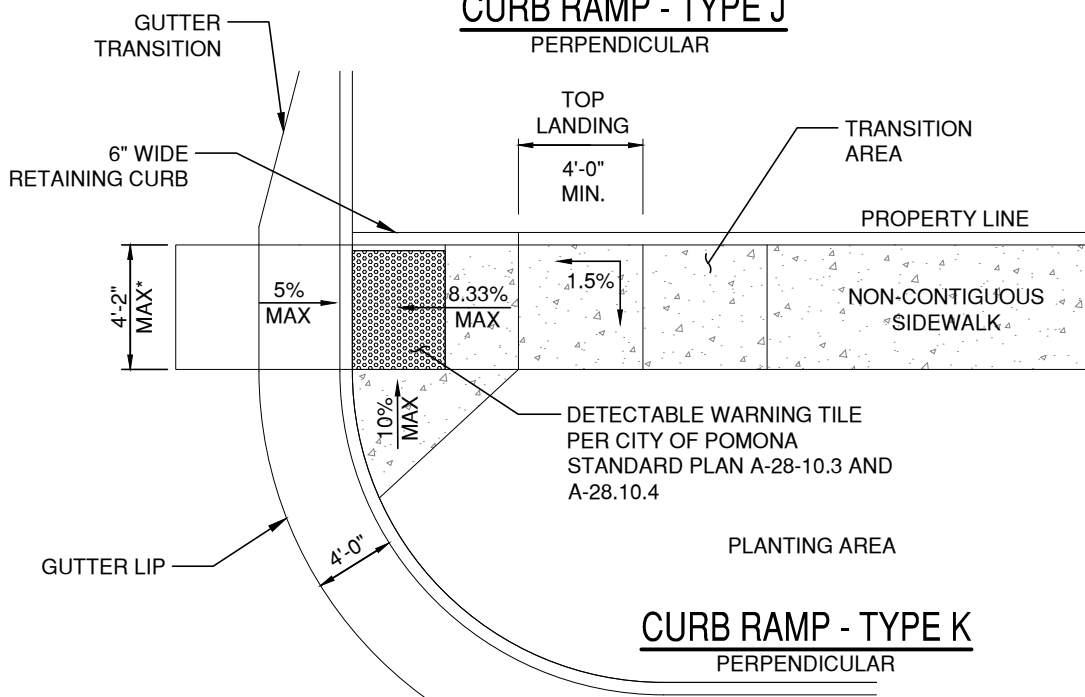
CITY OF POMONA  
 PUBLIC WORKS DEPARTMENT

**CURB RAMP TYPE I**

|   |  |      |         |                    |          |                |
|---|--|------|---------|--------------------|----------|----------------|
|   |  |      |         | DRAWN BY: _____    | STANDARD | 15<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____  |          |                |
| △ |  | DATE | INITIAL | APPROVED BY: _____ |          |                |



**CURB RAMP - TYPE J**  
PERPENDICULAR



**CURB RAMP - TYPE K**  
PERPENDICULAR

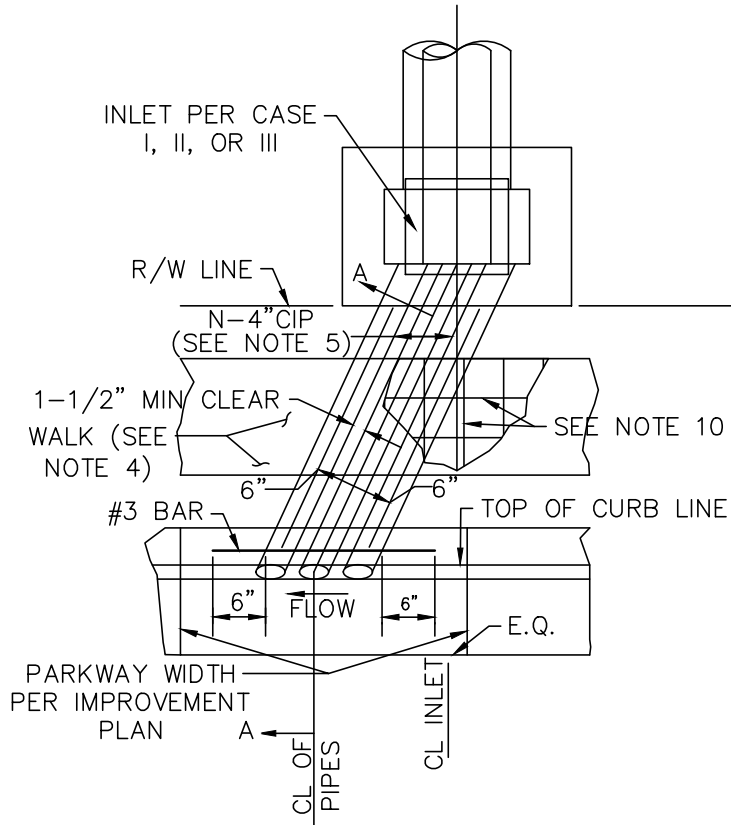
**NOTES:**

\* WIDEN THE CURB RAMP WIDTH TO 4'2" MAXIMUM TO ACCOMMODATE FORMWORK FOR THE 6" WIDE RETAINING CURB.

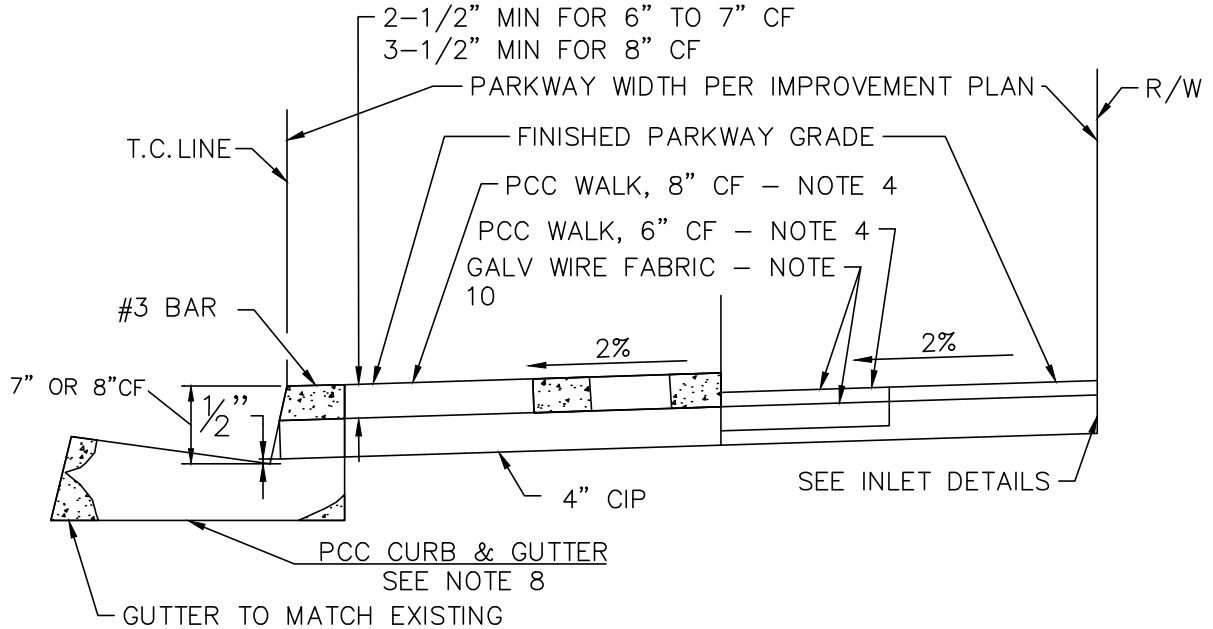
CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**CURB RAMP TYPE J, K**

|   |  |      |         |                    |          |                |
|---|--|------|---------|--------------------|----------|----------------|
|   |  |      |         | DRAWN BY: _____    | STANDARD | 16<br>OF<br>16 |
|   |  |      |         | CHECKED BY: _____  |          |                |
| △ |  | DATE | INITIAL | APPROVED BY: _____ |          |                |

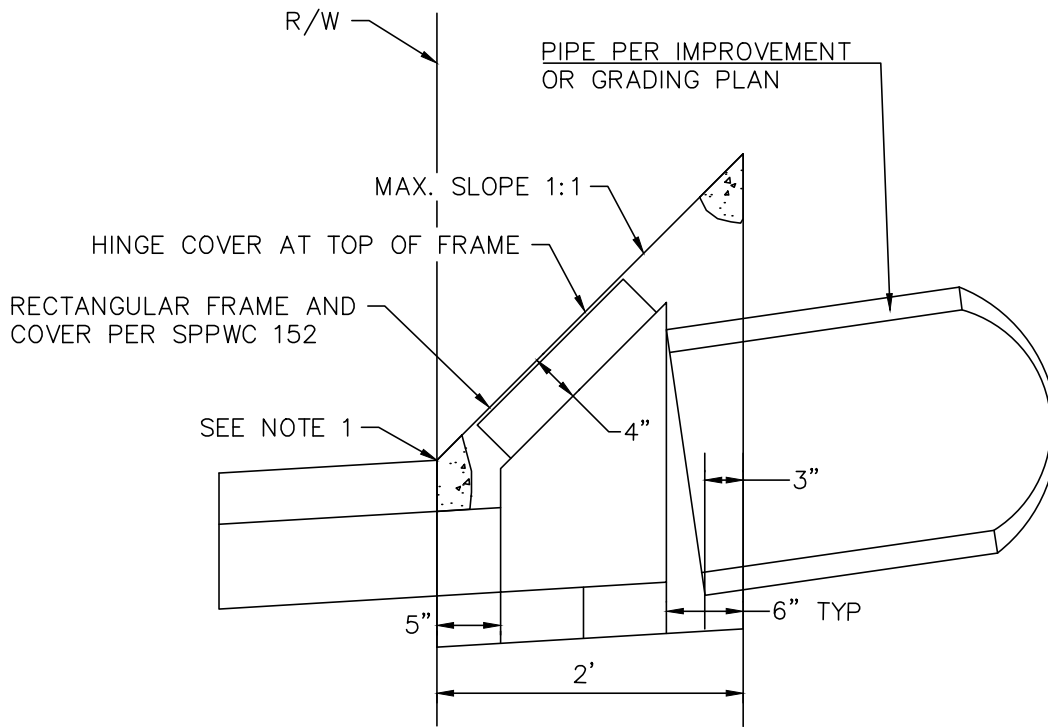


**CURB DRAIN PLAN**

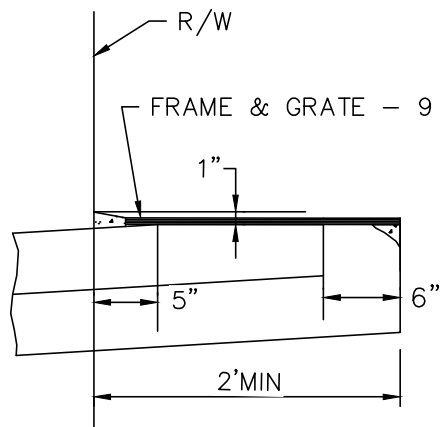


**SECTION A-A**

|                                           |  |      |         |                    |
|-------------------------------------------|--|------|---------|--------------------|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |  |      |         |                    |
| <b>CURB DRAIN</b>                         |  |      |         |                    |
|                                           |  |      |         | DRAWN BY: _____    |
|                                           |  |      |         | CHECKED BY: _____  |
|                                           |  |      |         | APPROVED BY: _____ |
| △                                         |  | DATE | INITIAL |                    |
|                                           |  |      |         | STANDARD           |
|                                           |  |      |         | 1<br>OF<br>4       |

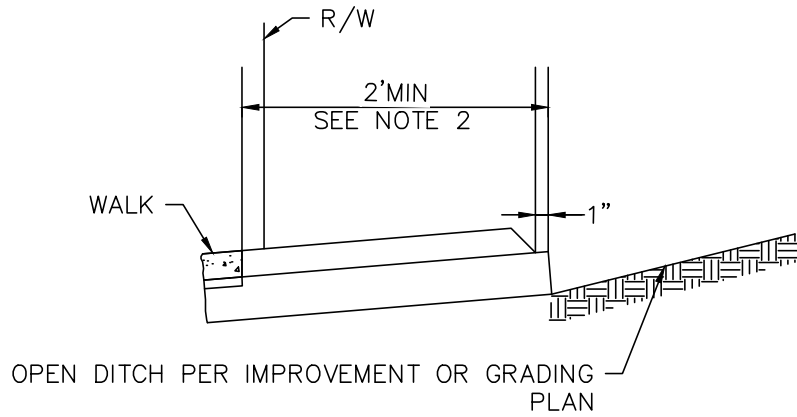


CASE I INLET  
TRANSITION STRUCTURE SECTION

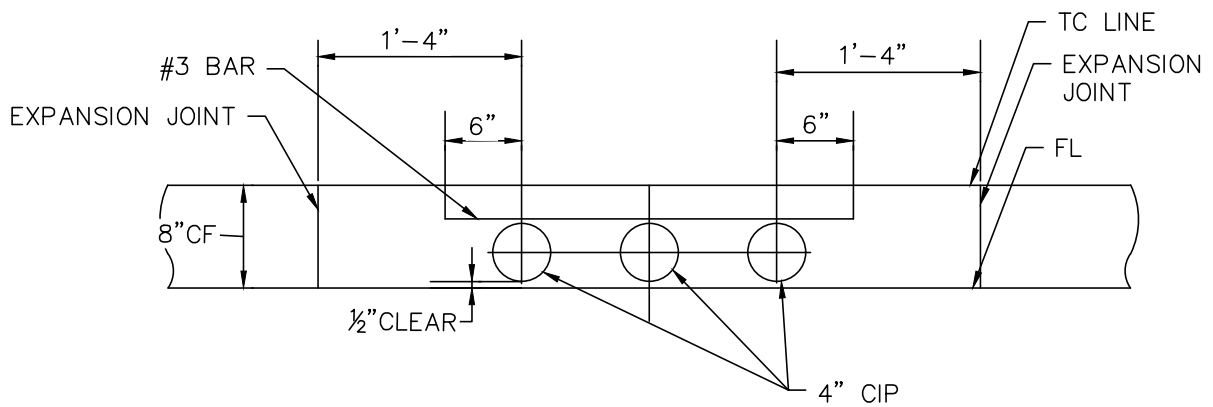


CASE II INLET  
DROP INLET CATCH BASIN SECTION

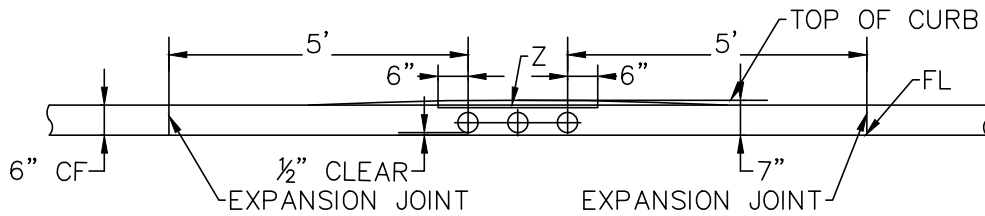
|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>CURB DRAIN</b>                         |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 2<br>OF<br>4 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |



**CASE III INLET**  
**GRADED DITCH SECTION**



**CURB PROFILE - 8" CURB FACE**  
**NOTE: APPLIES TO ANY NUMBER OF PIPES**

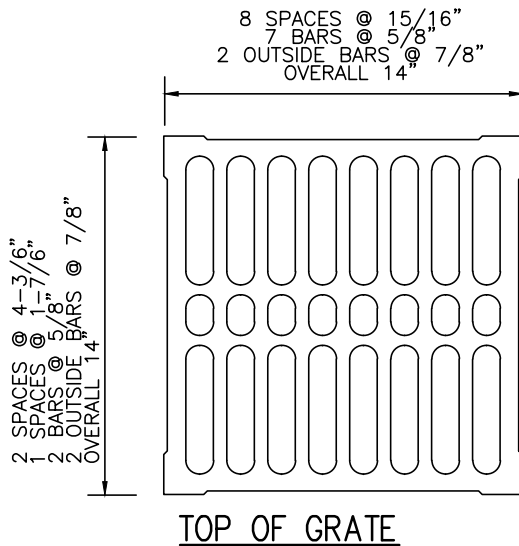


**CURB PROFILE - 6" CURB FACE**  
**NOTE: APPLIES TO ANY NUMBER OF PIPES**  
**Z = TOP-OF-CURB LINE SHOWN ON PROFILE**

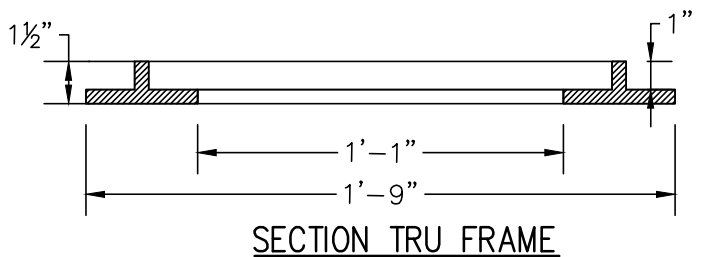
|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>CURB DRAIN</b>                         |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 3<br>OF<br>4 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |

**NOTES:**

1. IF THE TOP OF SLOPE IS ALLOWED WITHIN THE R/W, INLET CASE I BEGINS AT THE TOP RATHER THAN THE R/W LINE.
2. FOR OPEN DITCH (CASE INLET III). THE 24" EXTENSION BEYOND THE R/W LINE IS NOT REQUIRED WHEN BACK OF WALK IS 24" OR MORE FROM THE R/W LINE; HOWEVER, PIPE SHALL EXTEND TO R/W LINE.
3. TOP OF INLET STRUCTURE (CASE I AND II) TO BE FLUSH WITH ADJACENT SURFACE WHERE PRACTICAL.
4. CONSTRUCT PCC WALK WHEN SPECIFIED ON PLANS. THE CONTRACT PRICE PAID FOR PCC WALK ITEM SHALL INCLUDE WALK CONSTRUCTED IN CONJUNCTION WITH PARKWAY CULVERT.
5. "N" EQUALS NUMBER OF PIPES (MAXIMUM OF THREE) AS SPECIFIED ON PLANS.
6. INLET CASE TO BE SPECIFIED ON PLANS.
7. ANGLE A EQUALS 0, UNLESS OTHERWISE SPECIFIED.
8. TYPE, DIMENSIONS AND ELEVATIONS OF P.C.C CURB AND GUTTER PER PLANS.
9. UNLESS OTHERWISE SPECIFIED, FRAME AND GRATE FOR CASE II INLET SHALL BE GALVANIZED CAST IRON. WEIGHT OF FRAME AND GRATE SHALL BE 80 LBS.
10. AT LOCATIONS WITH LESS THAN 8" CURB FACE, USE 6x6-10/10 GALVANIZED WIRE FABRIC. WIRE FABRIC SHALL EXTEND 8" BEYOND THE EDGE OF CAST IRON PIPES.
11. ALL DRAINS MUST HAVE FLO-GUARD FILTER ON-SITE BEFORE DISCHARGING INTO CITY RIGHT-OF-WAY, PER SUSMP REQUIREMENTS.

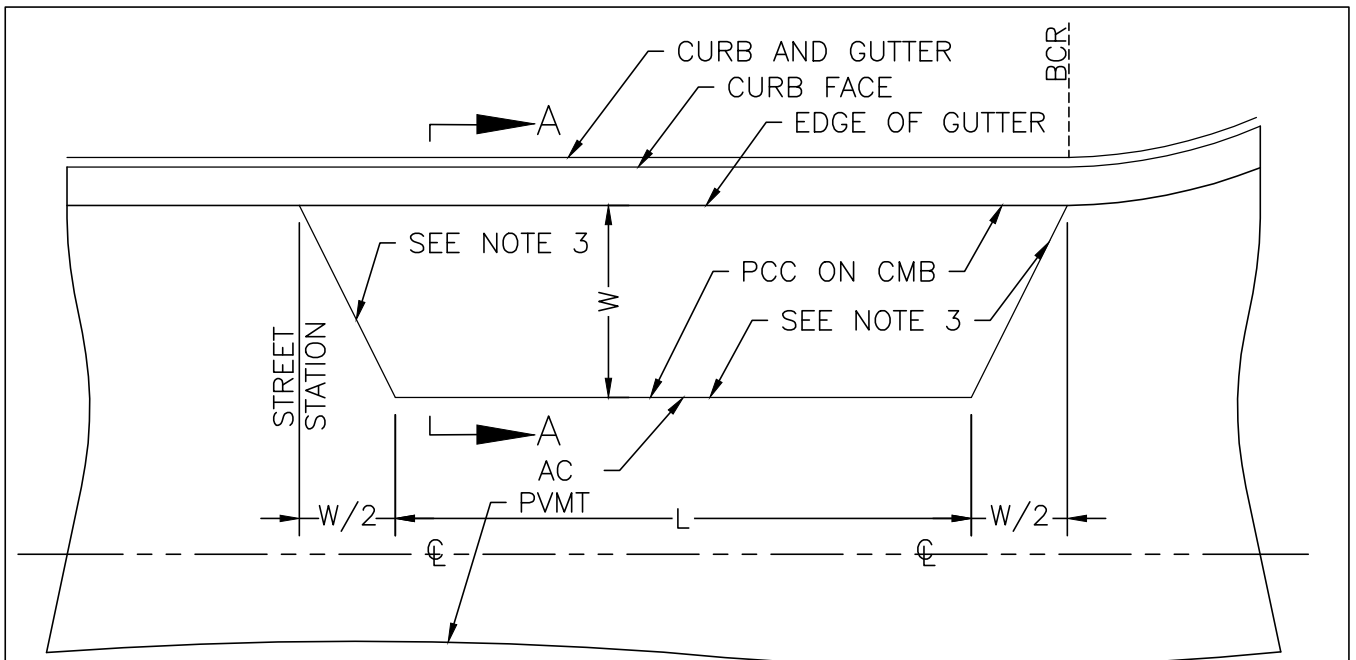


OPEN AREA 52%

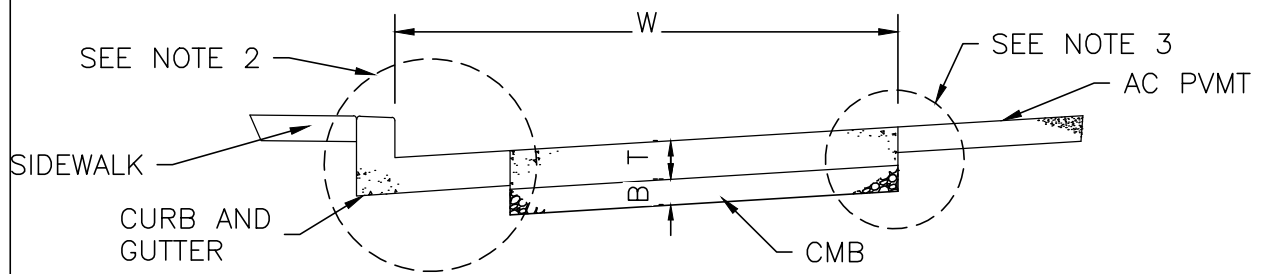


**GRATE FOR CASE II INLET**

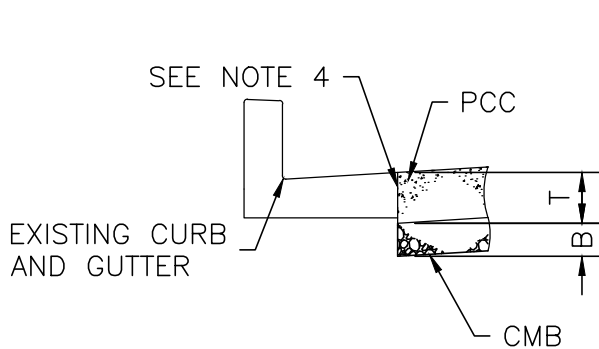
|   |  |      |         |                                           |          |    |
|---|--|------|---------|-------------------------------------------|----------|----|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |    |
|   |  |      |         | <b>CURB DRAIN</b>                         |          |    |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 4  |
|   |  |      |         | CHECKED BY: _____                         |          | OF |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          | 4  |



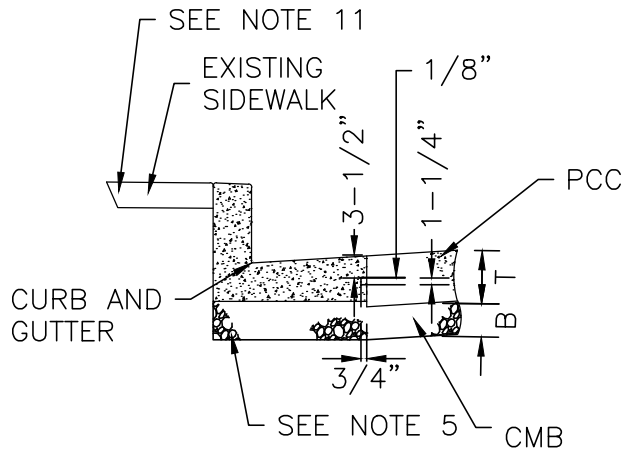
**TYPICAL BUS PAD**



**SECTION A-A**



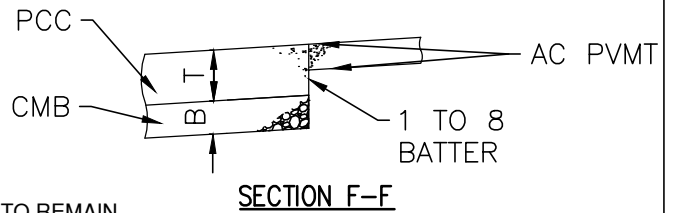
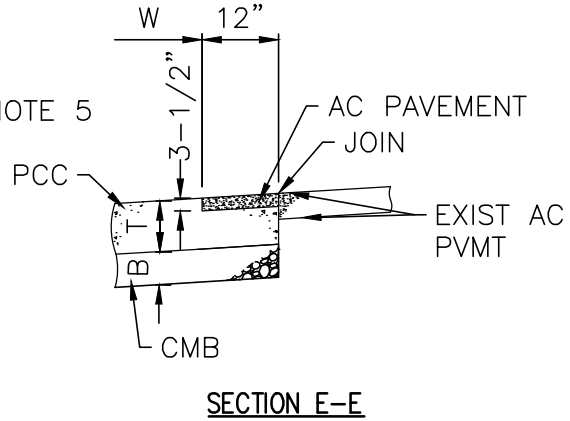
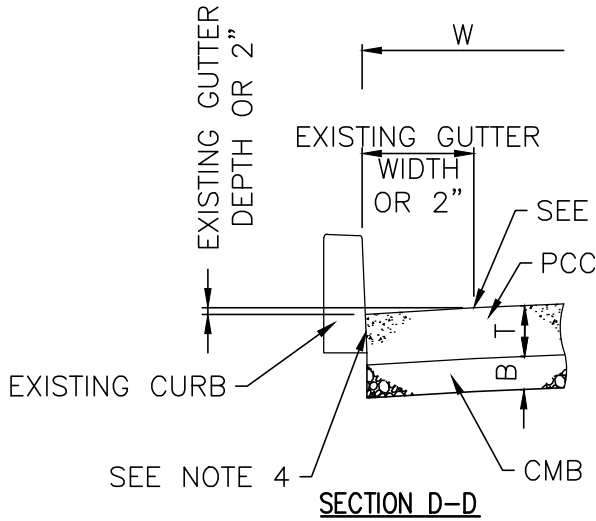
**SECTION B-B**



**SECTION C-C**

|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>CONCRETE BUS PAD</b>                   |          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD | 1<br>OF<br>2 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
|   |  |      |         | APPROVED BY: _____                        |          |              |
| △ |  | DATE | INITIAL |                                           |          |              |





**NOTES:**

1. DIMENSIONS: (UNLESS OTHERWISE SHOWN)

L= 85' T=8"  
W= 10' B= 6"

2. USE SECTION B-B FOR EXISTING CURB AND GUTTER THAT IS TO REMAIN.  
USE SECTION C-C FOR NEW CURB AND GUTTER.  
USE SECTION D-D FOR EXISTING CURB THAT IS TO REMAIN.
3. USE SECTION E-E FOR EXISTING AC PAVEMENT.  
USE SECTION F-F FOR NEW AC PAVEMENT.
4. AT LOCATIONS WHERE PCC PAVEMENT WILL ABUT EXISTING CONCRETE AN EPOXY APPROVED BY THE ENGINEER SHALL BE APPLIED TO THE EXISTING CONCRETE SURFACES PRIOR TO CONCRETE PLACEMENT.
5. CONSTRUCT LONGITUDINAL WEAKENED-PLANE JOINT TO MATCH ADJOINING EXISTING GUTTER WIDTH, OR 2' IF NO ADJOINING GUTTER EXISTS.
6. USE 2"X4" (50X100) HEADER TO FORM 3-1/2" STEP. TOP OF HEADER SHALL BE SET TO LINE AND GRADE.
7. ALL EXPOSED PCC CORNERS SHALL BE ROUNDED WITH A 1/2" RADIUS.
8. SURFACE OF CONCRETE SHALL HAVE A ROUGH TRANSVERSE BROOM FINISH.
9. WHERE DESIGNATED BY THE ENGINEER UNDESIRABLE SUBGRADE MATERIAL SHALL BE REMOVED AND REPLACED WITH CMB.
10. WHERE NEW CURB AND GUTTER IS CONSTRUCTED ADJACENT TO EXISTING SIDEWALK, SIDEWALK SHALL BE REMOVED AND REPLACED TO NEAREST SCORELINE.
11. CONSTRUCT TRANSVERSE WEAKENED PLANE JOINTS IN BUS PAD PAVEMENT APROX. 10' INTERVALS.
12. CONSTRUCT TRANSVERSE WEAKENED PLANE JOINTS IN BUS PAD PAVEMENT AT ALL EXISTING CURB/CURB & GUTTER CONSTRUCTION JOINTS AND WEAKENED-PLANE JOINTS.
13. AT THE OPINION OF THE ENGINEER THE EXISTING PAVEMENT MAY BE NEATLY SAWCUT AROUND THE DIMENSIONS OF THE BUS PAD, AND CONCRETE POURED DIRECTLY USING THE EXISTING PAVEMENT AS A FORM. THE CONCRETE EDGES SHALL BE ROUNDED WITH A 1/2" RADIUS.

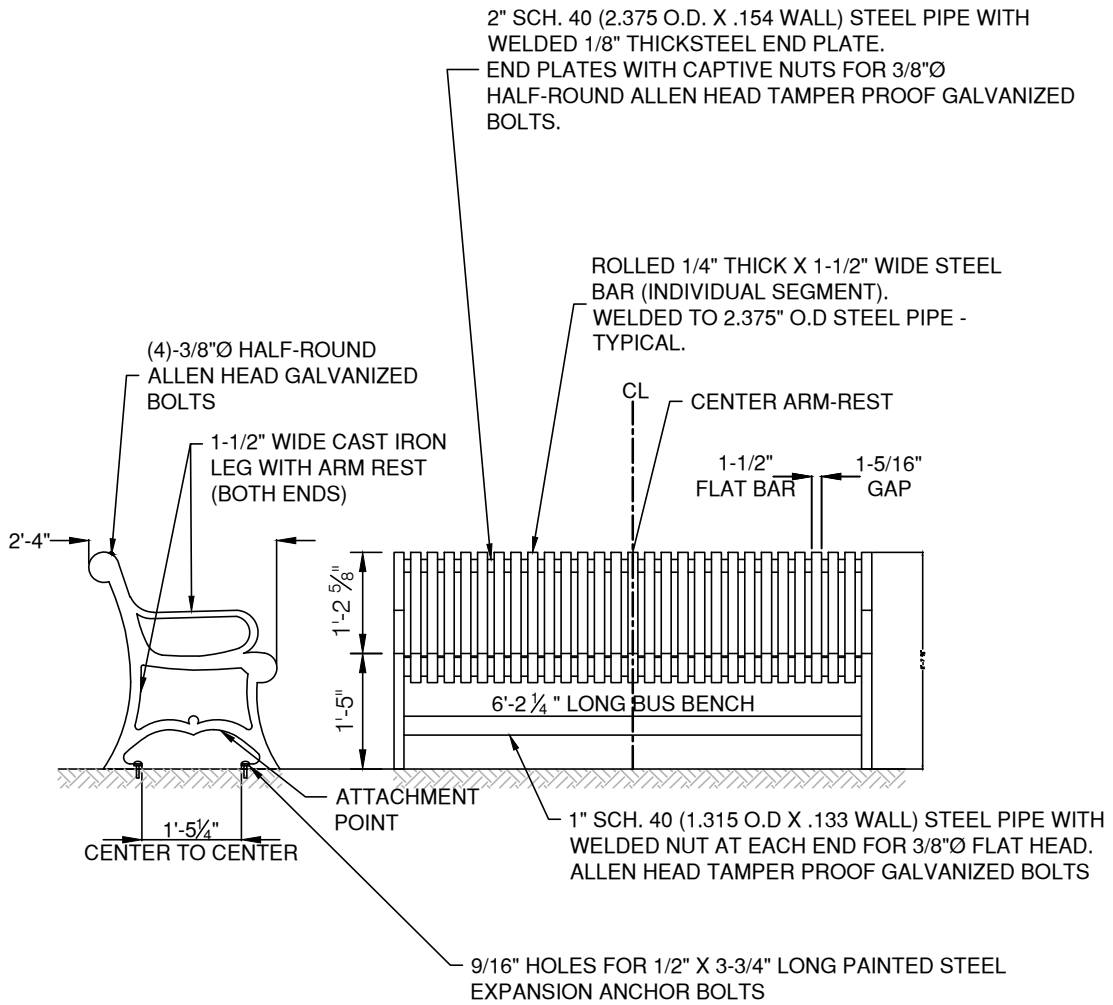
14. P.C.C. MIX DESIGN SHALL BE AS APPROVED BY ENGINEER.

15. CURING COMPOUND SHALL BE PROVIDED FOR ALL P.C.C.

CITY OF POMONA  
PUBLIC WORKS DEPARTMENT

**CONCRETE BUS PAD**

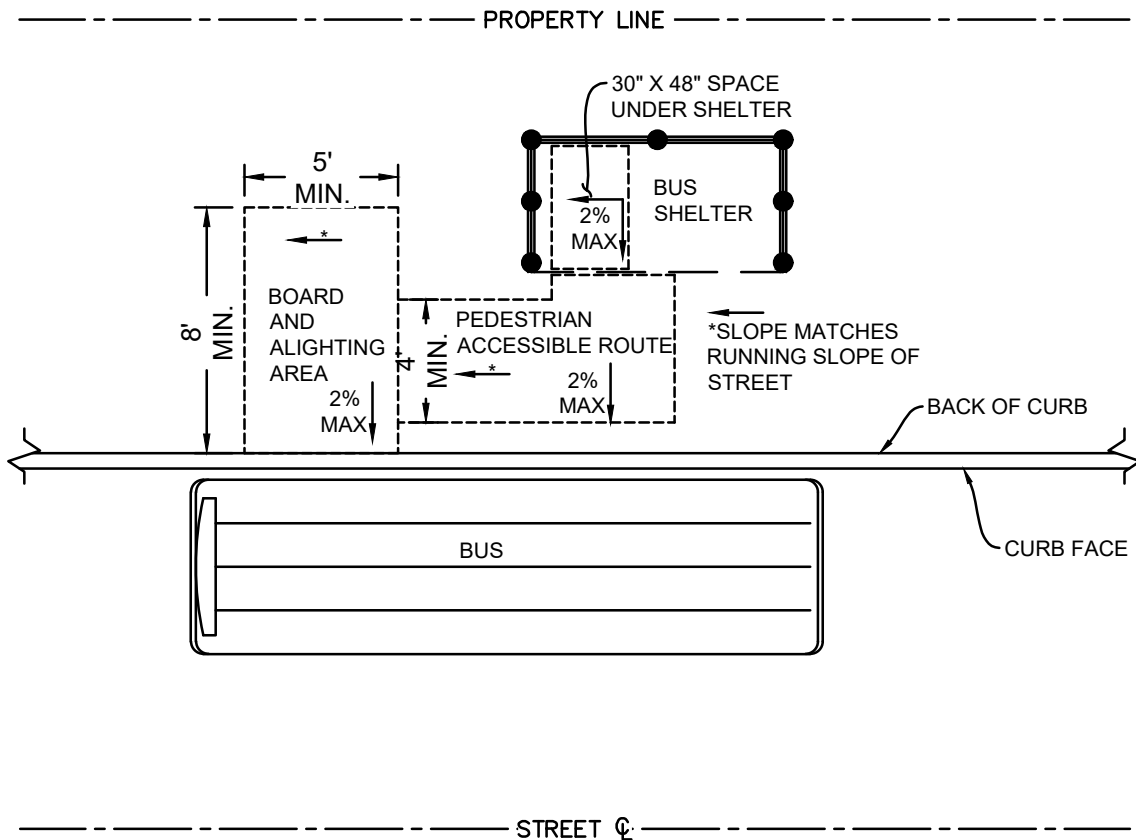
|   |  |      |         |                    |          |
|---|--|------|---------|--------------------|----------|
|   |  |      |         |                    |          |
|   |  |      |         |                    |          |
|   |  |      |         | DRAWN BY: _____    | STANDARD |
|   |  |      |         | CHECKED BY: _____  |          |
| △ |  | DATE | INITIAL | APPROVED BY: _____ |          |
|   |  |      |         |                    | 2 OF 2   |



**NOTES:**

1. ALL STEEL MEMBERS COATED W/ZINC RICH EPOXY THEN FINISHED W/POLYESTER POWDER COATING.
2. PROVIDE A 30" X 48" WHEEL CHAIR SPACE TO BE LOCATED ADJACENT TO BENCHES LOCATED COMPLETELY UNDER BUS SHELTER (IF PROVIDED).

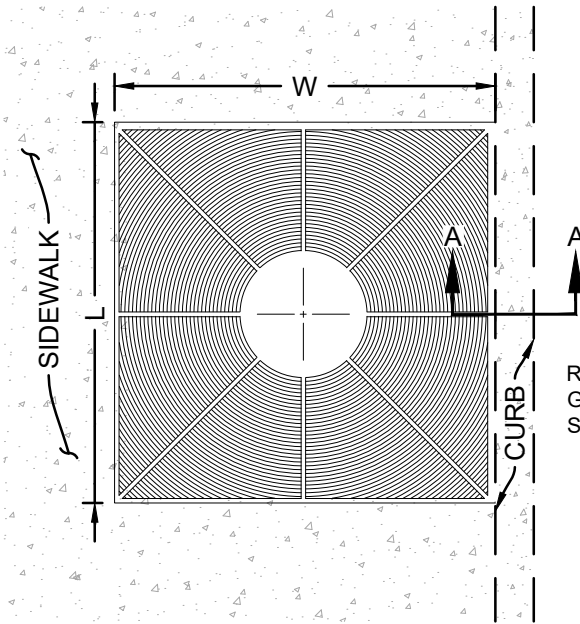
|   |  |      |         |                                           |              |
|---|--|------|---------|-------------------------------------------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |              |
|   |  |      |         | <b>BUS BENCH</b>                          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD     |
|   |  |      |         | CHECKED BY: _____                         |              |
|   |  |      |         | APPROVED BY: _____                        |              |
| △ |  | DATE | INITIAL |                                           | 1<br>OF<br>2 |



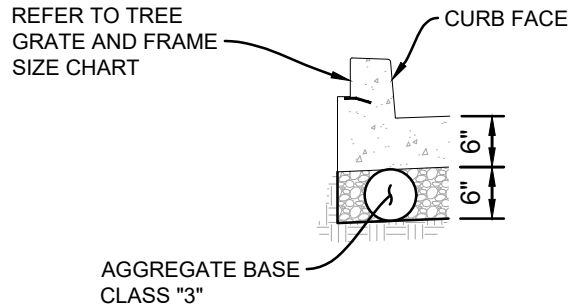
**NOTES:**

1. PROVIDE A 30" X 48" WHEEL CHAIR SPACE TO BE LOCATED ADJACENT TO BENCHES LOCATED COMPLETELY UNDER BUS SHELTER (IF PROVIDED).
2. PROVIDE A 5' BY 8' BOARDING AND ALIGHTING AREA ADJACENT TO CURB.
3. PROVIDE A 4' WIDE MIN ACCESSIBLE PEDESTRIAN ROUTE FROM THE CLEAR FLOOR SPACE UNDER BUS SHELTER TO THE BOARDING AND ALIGHTING AREA.

|   |  |      |         |                                           |          |              |
|---|--|------|---------|-------------------------------------------|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |              |
|   |  |      |         | <b>BUS SHELTER DETAIL</b>                 |          |              |
|   |  |      |         | DRAWN BY:                                 | STANDARD | 2<br>OF<br>2 |
|   |  |      |         | CHECKED BY: _____                         |          |              |
| △ |  | DATE | INITIAL | APPROVED BY: _____                        |          |              |



| TREE GRATE AND FRAME SIZE CHART |                    |
|---------------------------------|--------------------|
| SIDEWALK WIDTH                  | FRAME SIZE (W X L) |
| 10' OR WIDER                    | 5' X 5'            |
| LESS THAN 10'                   | 4' X 4'            |



PLAN

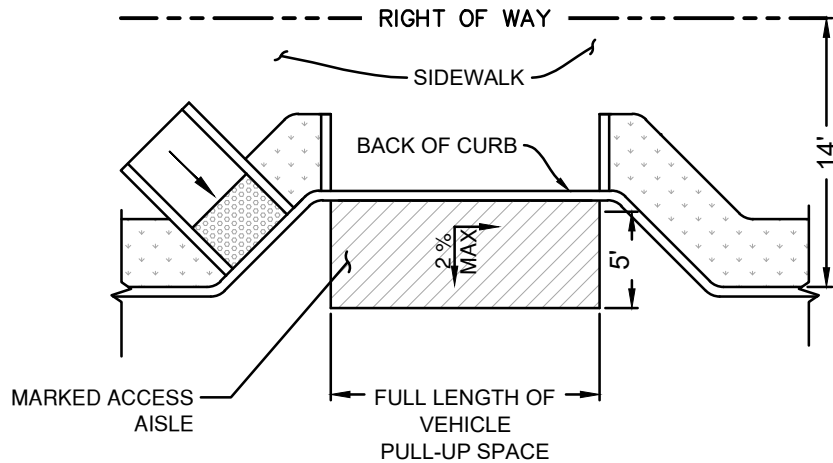
SECTION A-A

TREE WELL DETAIL

NOTES:

1. CONCRETE SHALL BE CLASS 3 AND SHALL BE MONOLITHIC WITH CURB, GUTTER AND SIDEWALK.
2. TREE WELL SHALL HAVE THE SAME SLOPE AS THE SURROUNDING SIDEWALK.
3. INSPECTION OF TREE WELLS IS REQUIRED. PRIOR NOTICE OF 48 HOURS SHALL BE GIVEN TO THE CITY ENGINEER WHEN REQUESTING INSPECTION.
4. WHEN CONSTRUCTING TREE WELL AROUND EXISTING TREE, TREE SHALL BE CENTERED WITH RESPECT TO THE "L" DIMENSION.
5. TREE WELL SHALL BE SQUARE (SIDES PARALLEL AND CORNERS 90°).
6. TREE GRATE SHALL HAVE A RADIAL PATTERN WITH OPENINGS EXPANDABLE TO ACCOMODATE INCREASING TRUNK DIAMETER.
7. TREE GRATE SHALL BE DUCTILE CAST IRON AND TWO PIECES.
8. OPENINGS IN THE TREE GRATE SHALL BE ADA COMPLIANT. (SLOTS SIZING 1/2" MAX PER LATEST ADA SECTION 302.3)

|   |  |      |         |                                           |              |
|---|--|------|---------|-------------------------------------------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |              |
|   |  |      |         | <b>TREE WELL</b>                          |              |
|   |  |      |         | DRAWN BY: _____                           | STANDARD     |
|   |  |      |         | CHECKED BY: _____                         |              |
|   |  |      |         | APPROVED BY: _____                        |              |
| △ |  | DATE | INITIAL |                                           | 1<br>OF<br>1 |

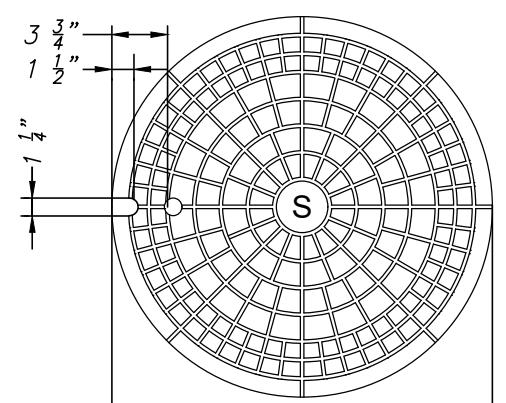
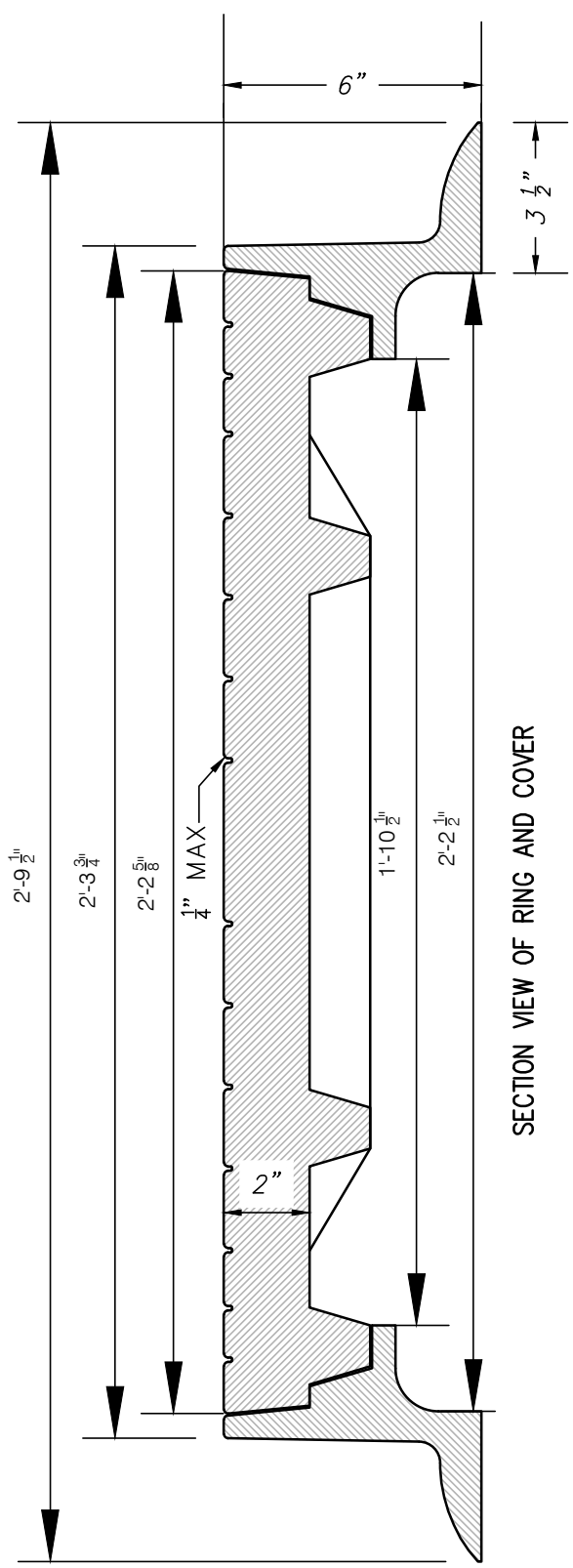


## PASSENGER DROP-OFF AND LOADING

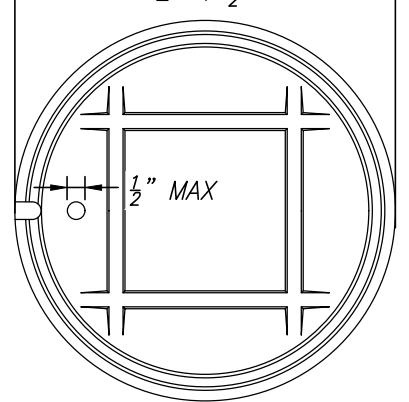
**NOTES:**

1. PASSENGER DROP-OFF AND LOADING ZONES SHALL PROVIDE A VEHICULAR PULL UP SPACE 5' WIDE MINIMUM AND 20' LONG MINIMUM.
2. PROVIDE AN ACCESS AISLE PARALLEL TO THE VEHICULAR PULL-UP SPACE, 5' WIDE AND EXTEND THE FULL LENGTH OF VEHICULAR PULL-UP SPACE.
3. ACCESS AISLE SHALL BE MARKED WITH A PAINTED BLUE BORDERLINE AROUND THEIR PERIMETER. THE AREA WITHIN BORDERLINES SHALL BE MARKED WITH HATCHED LINES A MAXIMUM OF 36 INCHES ON CENTER IN A COLOR CONTRASTING WITH THAT OF THE AISLE SURFACE.

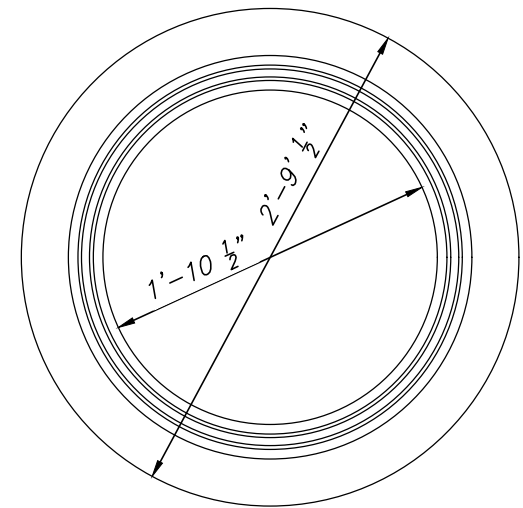
|   |  |      |         |                                           |  |          |              |
|---|--|------|---------|-------------------------------------------|--|----------|--------------|
|   |  |      |         | CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |  |          |              |
|   |  |      |         | <b>PASSENGER DROP-OFF AND LOADING</b>     |  |          |              |
|   |  |      |         | DRAWN BY: _____                           |  | STANDARD | 1<br>OF<br>1 |
|   |  |      |         | CHECKED BY: _____                         |  |          |              |
|   |  |      |         | APPROVED BY: _____                        |  |          |              |
| △ |  | DATE | INITIAL |                                           |  |          |              |



PLAN OF COVER



BOTTOM VIEW



PLAN OF RING

SECTION VIEW OF RING AND COVER

APPROX. WEIGHT = FRAME 260 LBS  
COVER 200 LBS

- NOTES-
1. MANHOLE FRAME AND COVER SHALL BE ALHAMBRA FOUNDRY NUMBER A-1170 OR EQUAL.
  2. MANHOLE FRAME AND COVER CASTING SHALL CONFORM TO CLASS 10 SERIAL NUMBER A-48 OF A.S.T.M.

|                                           |          |  |              |
|-------------------------------------------|----------|--|--------------|
| CITY OF POMONA<br>PUBLIC WORKS DEPARTMENT |          |  |              |
| <b>SEWER MANHOLE FRAME &amp; COVER</b>    |          |  |              |
| DRAWN BY: _____                           | STANDARD |  | 1<br>OF<br>1 |
| CHECKED BY: _____                         |          |  |              |
| APPROVED BY: _____                        |          |  |              |
| DATE                                      | INITIAL  |  |              |

|   |  |  |  |  |  |  |  |
|---|--|--|--|--|--|--|--|
| △ |  |  |  |  |  |  |  |
|---|--|--|--|--|--|--|--|